

Haverhill South BMX Pump Track – Greenfield Way

Haverhill South People's Forum is part of the Local Conversations programme, an innovative funding approach developed by People's Health Trust who work to ensure that where people live does not unfairly reduce the length or quality of their life. People's Health Trust is an independent charity addressing health inequalities across Great Britain. It works closely with each of the 51 society lotteries and makes grants using money raised by the society lotteries through The Health Lottery. The Local Conversation in Haverhill South is funded using money raised through The Health Lottery to address health inequalities in Suffolk'.

Delivered in partnership with Community Action Suffolk, Haverhill South People's Forum is being supported to develop aspirations, priorities and identify projects for their community.

As our keynote project, we are looking for an excellent company to construct a BMX Pump / practice track according to our design and consultation with local users.

Specification:

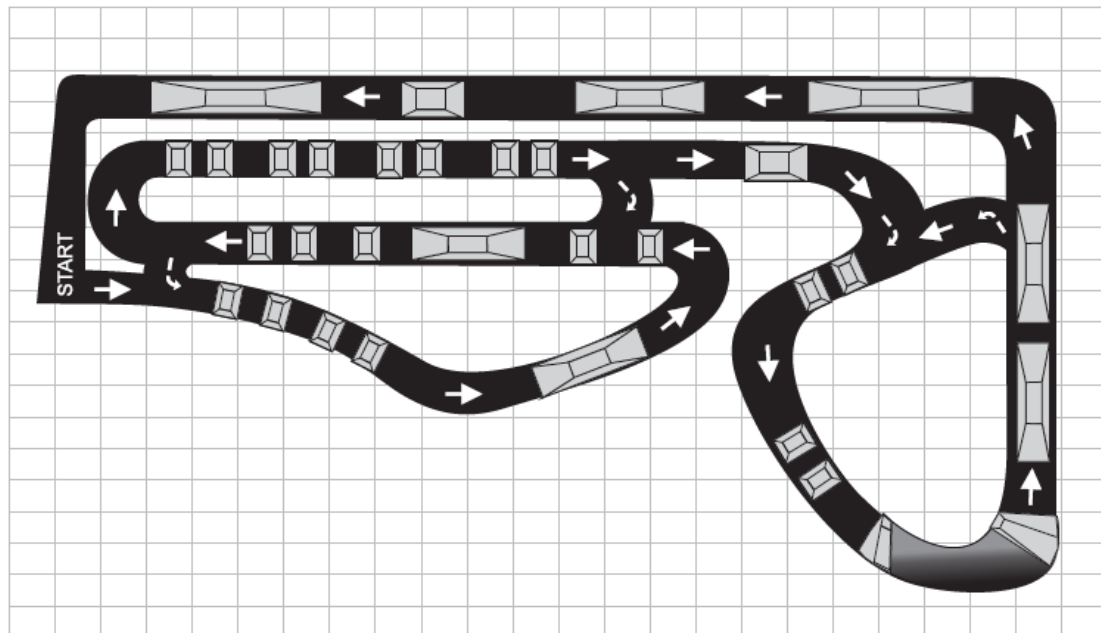
- **Construction of BMX Pump track according to attached design and following specification points:**
- **Installation of sufficient drainage**
- **Track width of 1.5 Metres**
- **100mm depth of MOT 1 Compact Sub base with a 50mm compact tarmac surface**
- **Tarmac Berms with 100mm of MOT 1 Compact Sub base with a minimum of 50mm compact tarmac surface**
- **Allow for excavations and back fill of trenches with insertion of conduits for lighting cabling in liaison with electrical contractors**
- **All equipment and works must conform to current BMX track health and safety standards**

Haverhill South Pump Track – Element Description:

- **Start hill:** This to be a small raised area, where the users reach the track and tend to collect between laps. It would be approximately 6m x 3m on top and be surfaced in Bitmac.
- **Berms:** Track site design to include one main 90 degree Berm, one main 180 degree Berm, as well as a section of built into the start hill. All Berms should be surfaced in Bitmac, this is considered to be the best surface on these otherwise high erosion areas. The outside of the Berms and Start hill should be landscaped with topsoil.
- **Profiled Straights:** The straights to be a series of rounded profiles (bumps) with landscaped sides. The straights will be surfaced with Tarmac, as are the Berms. Please note all quotes would benefit from engagement for users group within Haverhill and will be invited to a consultation in Haverhill.
- The provision design of the 'Bumps' to include the following: pair of rollers, pick double, step up, low triple, long camel, large low roller
- **Landscaped areas:** Once complete, the infield and slopes adjacent to the track surface would be landscaped areas and would ultimately be grassed
- **User Access:** Users would generally reach the track via the existing pathway off Greenfield Way. A small section of the path will be required to link the existing pathway at ground level to the level of the Start hill.

Proposed Track layout

Proposed Haverhill Pump Track Layout



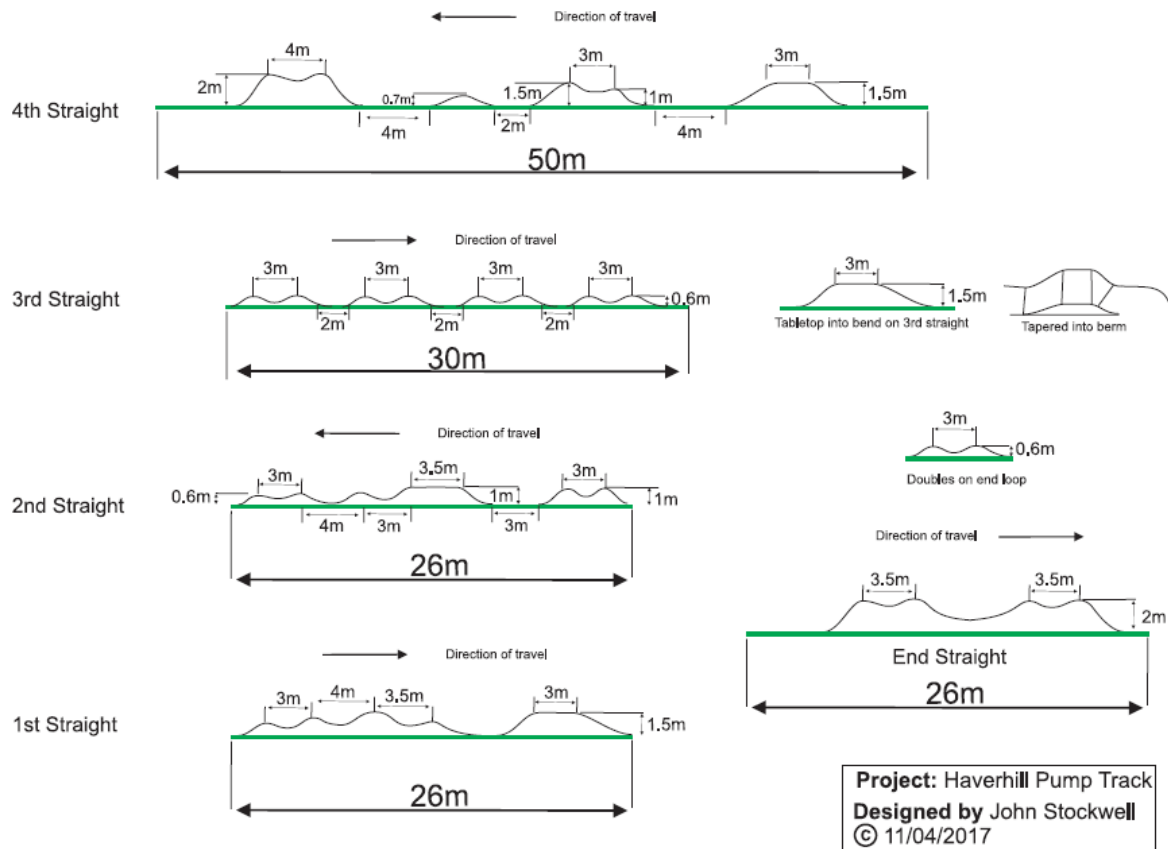
RideSmart / ATB Coaching

Project: Haverhill Pump Track
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Drainage:

- Efficient drainage to dissipate surface water should be provided; it's preferable if the track is simply built 'high and dry' above the surrounding levels, with over land drainage.

The chosen contractor will be selected according to previous experience within the field. The chosen contractor will need to use their expertise to improve the proposed design to create the best possible facility for the users.

Your budget should be based on the above design only, include for preparatory ground works, protection and security of the site and works until handover, supply and installation of equipment and minor reinstatement works.

Timeframe:

Bidders are required to include a timeframe (which may be in the form of a Gantt Chart) setting out the timetable for works. For reference, the contract is for delivery of the completed track by June 2018.