

INVITATION TO TENDER RIVERBANK REPAIRS AT MILL AND MARSH MEADOWS, HENLEY-ON-THAMES, OXON

1. Introduction

- 1.1 You are invited to submit a quote for repairs and improvement works to the river frontage at Mill and Marsh Meadows, the riverside park, owned by Henley-on-Thames Town Council (RG9 1BE). This is an important asset for the Council, drawing visitors to the town and bringing significant annual income from its temporary and permanent moorings.

2. Background information

- 2.1 The river frontage consists of 3 sections:

Marsh Meadows – 470m temporary non-residential moorings

Mill Meadows – 330m temporary non-residential moorings

Mill Meadows – 127m permanent non-residential moorings

- 2.2 The nature of the riverbank varies along these three stretches, depending on natural features, such as the height of the bank above river level, and man-made features including the river wall construction and associated works.
- 2.3 There has been no major, comprehensive work on the riverbank for a number of years, although some work has been carried out to specific sections.

3. Scope of work

- 3.1 A riverbank survey undertaken in September 2020 identified the repairs and improvements required to ensure the long-term structural soundness of the river frontage. Background summary information including a location map is given in Appendix 1.

4. Anticipated value

- 4.1 The overall value of the recommended work is estimated to be in excess of £300K. It is expected that the construction of a new river wall will be undertaken in sections to ensure the availability of moorings at all times. For comparative purposes, quotes for 25 m sections are requested with any provisos and cost implications listed for the works, being broken down into sections.

5. Specification

5.1 You are asked to provide itemised quotes, using the pricing schedule provided, for the following work identified in the survey:

5.2 Marsh Meadows

- i) To provide new steel piling at four sections of river bank of lengths 10.8 m, 7.8 m, 4.1 m and 2.2 m, where the riverbank is lower. The new steel piling to be installed in front of the existing piling to the same level as the adjacent piling, and the land behind levelled to suit the adjacent land.
- ii) To install a 1200 mm long single course of concrete sandbags to replace those that have become displaced.

5.3 Mill Meadows

- i) To construct a new river wall in 25 m sections, using one of the following methods:

Option A – new galvanized sheet steel piling, with hardwood timber capping and waling;

Option B – concrete and block construction.

Quotes should include:

- Transport of materials to site and set up;
- Fencing off a 25 m section as required;
- Breaking out concrete from behind existing sheet piling and removal from site for safe disposal;
- Refixing and refurbishing or providing new heavy-duty mooring rings at the temporary moorings as required;
- Retaining and if necessary, refurbishing the existing raised mooring posts at the permanent moorings;
- Repairing and filling with tarmac any resulting gap with the footpath;
- Clearing site on completion.

Itemised quotes per 25 m section are required for both Options A and B.

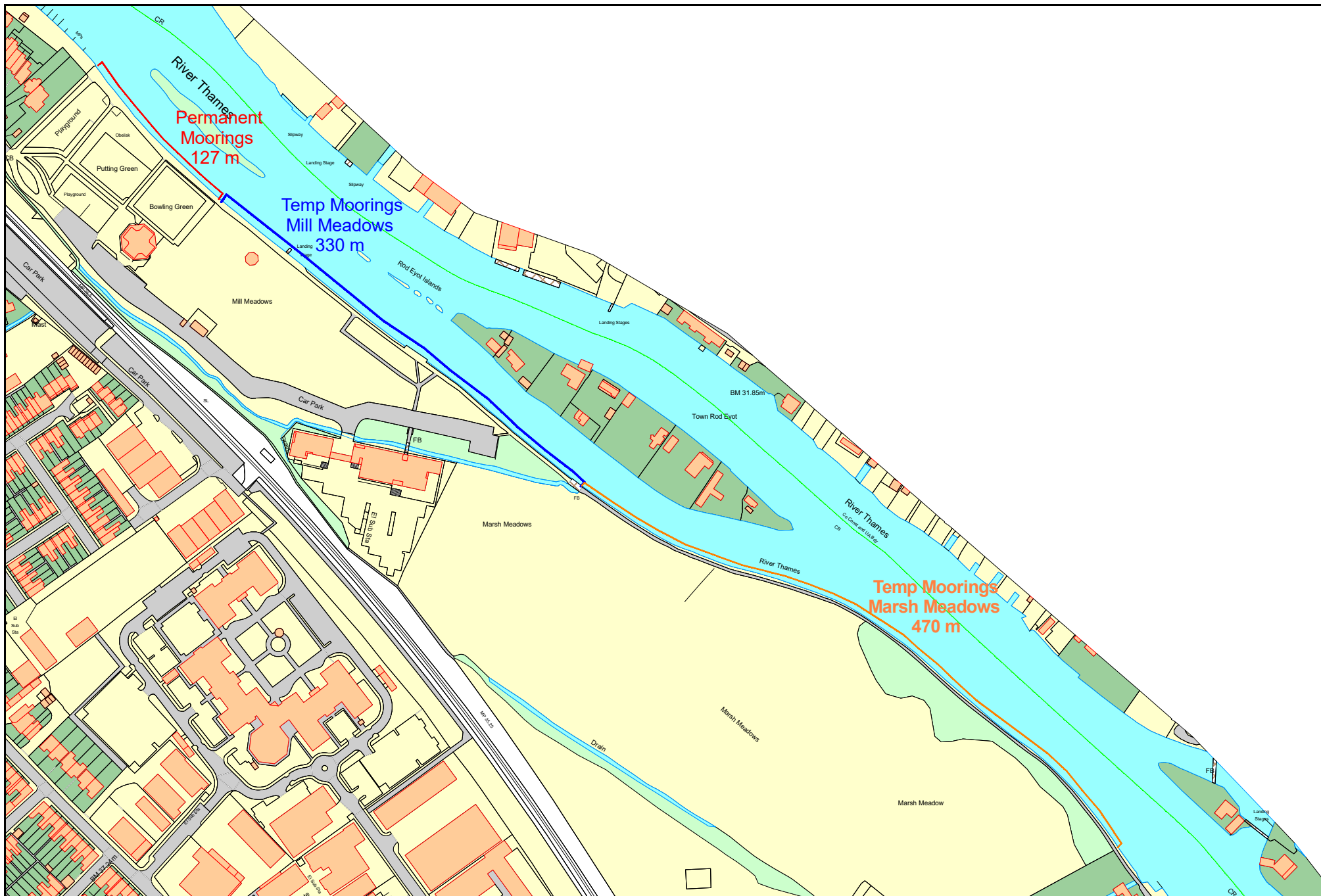
- ii) To repair the 8 m section of river wall under the River and Rowing Museum jetty which has slumped.
- iii) To install a ladder for exit from the river. Location to be close to the River and Rowing Museum landing stage but not conflicting with the moorings.

6. Tender process

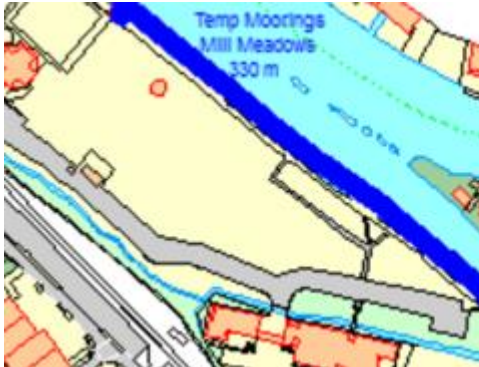


- 6.1 Tenders should be sent by email to Becky Walker, Estates Manager at b.walker@henleytowncouncil.gov.uk . Submissions must include the completed Pricing Schedule and the completed Tender Response Form for contractors.
- 6.2 Please ensure that any other information that has been requested to support your tender has been included. Failure to return all documents requested may be deemed as a non-compliant tender.
- 6.3 Schedules should be priced exclusive of VAT. Tenders must remain open for acceptance for a minimum of 60 days. The Council does not bind itself to accept the lowest or any quotation.
- 6.5 The successful bidder must not undertake any work without written notification that they have been awarded the contract.
- 6.6 **Closing date for tenders: Monday 1 February 2021.**
- 6.7 For further information or to arrange a site visit, please email:-
Karl Bishop, Parks Manager on k.bishop@henleytowncouncil.gov.uk or
Becky Walker, Estates Manager on b.walker@henleytowncouncil.gov.uk

7. Freedom of information

- 7.1 Information in relation to this quotation may be made available on demand in accordance with the requirements of the Freedom of Information Act 2000.
- 7.2 Bidders should state if any of the information supplied by them is confidential or commercially sensitive or should not be disclosed in response to a request for information under the Act. Bidders must provide justifications why they consider the information to be confidential or commercially sensitive and for how long.
- 7.3 Bidders acknowledge that neither an assertion nor the provision of justifications pursuant to the above clause constitutes a guarantee that the information will not be disclosed by the Council, pursuant to a valid request made under the Act.



Summary or Recommendations based on River Bank Survey 2020

Location - Insert maps refer to the map at end of summary.	Construction, condition and Issues	Suggested solutions
<p>Mill Meadows - Temporary Moorings</p> <ul style="list-style-type: none"> - 330 of non-residential moorings - maximum stay 14 nights on return within 7 nights 	<p>Construction – an original stone wall which has had sheet piling, back filled with concrete, on a lower level and which in places has been built up with concrete blocks (see images below).</p> <p>Condition - The wall and kerbing has become been damaged over the years and the materials have become brittle and degraded with time and by vegetation growing up behind the stone wall and between the stone wall and the piling. The general condition of the wall is sound apart for an 8 m section below the River and Rowing Museum jetty.</p> <p>Issues - a) The original stone wall and the lower sheet piling forms a step out and creates a wider gap and which can add to difficulty embarking and disembarking).</p> 	<p>A new river wall constructed in galvanised sheet piling would be the most suitable long-term solution providing a consistent, flat wall on which a new and uniform top edging could be placed. A hardwood timber capping (top) and/or waling (side) would make it more aesthetically pleasing from the towpath and the river, would provide a secure edge to stand on when embarking and disembarking and more forgiving than a hard stone, concrete or metal finish for boats to moor up against.</p>  <p><i>Image shows hard wood capping</i></p>

b) The top edge of the bank is damaged and does not provide an even surface to stand on when embarking and disembarking. The top edge is either the original stone wall or topped with concrete blocks and both have become degraded either due to parts breaking away for instance when heavy boats pull against the mooring rings, the material crumbling or general deterioration.



c) An 8 m section of the wall below the River and Rowing Museum jetty has slumped.



d) The river's edge looks piecemeal and unattractive.

e) Mooring rings are unevenly spaced and some are set away from the edge which can cause a trip hazard. Also, with increasingly larger and heavier boats some of the rings are not substantial enough.



Image shows hard wood capping and waling

OR

ii) A concrete and block wall could be considered and may be cheaper however could potentially damage boats and be damaged by heavy steel boats.

AND

lii) install new, heavy duty mooring rings at 5 m intervals close to or on the new edge.

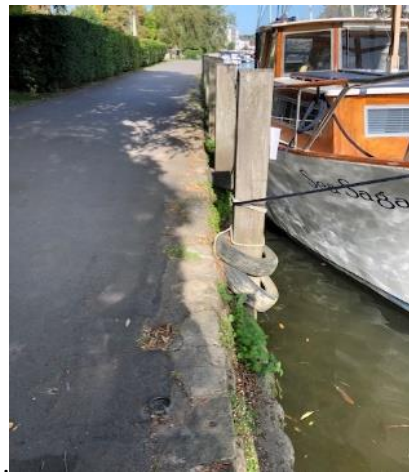
Mill Meadows – Permanent Moorings

- 127 m of non-residential moorings
- managed by Hobbs of Henley (current agreement until 2025)



Construction – the permanent moorings are of the same basic construction as the temporary moorings with the original stone wall which has had sheet piling back filled with concrete on a lower level. In addition, there are mooring posts with metal rods attached which mooring ropes can slide up and down to allow boats to rise and fall with the water levels.

Condition – The wall is in a similar condition to the temporary moorings with an even top edge. The mooring posts are in sound condition and could be incorporated into any new design and cosmetically improved.



Issues: - f) The lower wall creates a step out (which can make embarking and disembarking more difficult (as a) above).

g) The top edge of the bank is uneven and does not provide an even surface when embarking and disembarking (as b) above).

f) The river's edge looks piecemeal and unattractive (as d) above).

iv) The suggested works would be as for the temporary moorings regarding the construction of a new wall with uniform capping and waling.

v) Retain and refurbish the mooring posts and new mooring rings as required.

To note: consultation with Hobbs of Henley who manage these moorings to be undertaken.

Marsh Meadows – Temporary Moorings

- 470 m of non-residential mooring
- maximum stay 14 nights on return within 7 nights



Construction - the majority of the river bank is steel sheet piling (456 m) installed over a number of years probably to replace the river wall when it started to fail. The bank is of variable height between 600 and 900 mm above normal river levels.

Condition - Most of the sheet piling has a steel angle iron running along the top and in others concrete sand bags have been placed on top of the sheet piling. Although some is likely to be at least 30 years old the steel piling is still in a reasonable condition. The piling is not the most attractive to view from the river however this area has a more rural feel and it is not a major eye-sore – future consideration could be given to recladding for aesthetic reasons.

Issues: - g) As the river bank varies in height along this stretch and the sheet piling has been installed at different times with either an angle iron or sand bags on top this has led to works being required in 4 sections as follows: -

- a 10.8m section of river bank which is lower does not have the steel angle iron placed on the top of the sheet piling and the adjacent higher sections of bank on either end do – this means the angle irons are exposed and could cause a hazard.

- In addition, there are 3 more sections as above of 7.8m, 4.1m and 2.2 m – see images below.



h) Sand bags have become displaced in a 1200 mm section.

iv) For (g) install sections of steel piling in front of the existing piling to the same level as the adjacent existing piling. The bank behind can then be filled, levelled and grassed.

v) For h) - add a single course of sand bags to a 1200 mm section where they have become displaced.

