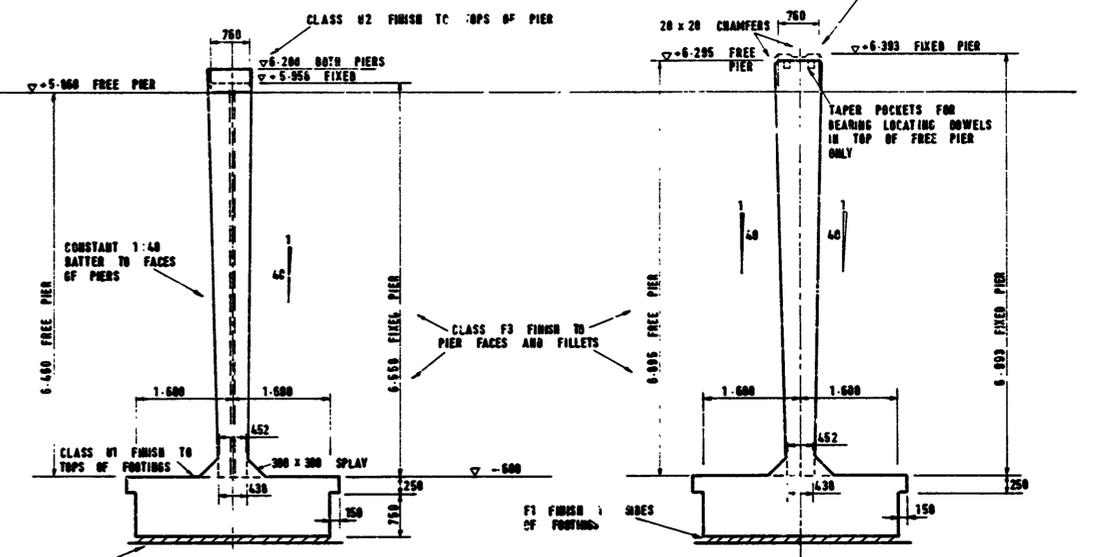


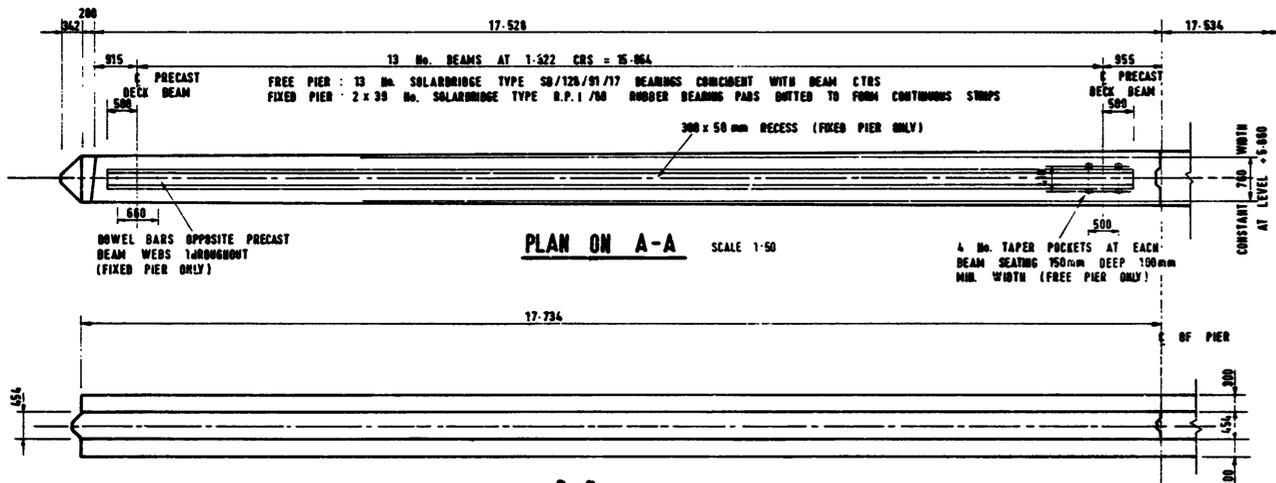
ELEVATION OF PIERS SCALE 1:50

FIXED AND FREE PIERS ARE IDENTICAL EXCEPT FOR PIER HEIGHTS AND BEARING DETAILS AS NOTED



C-C SCALE 1:50

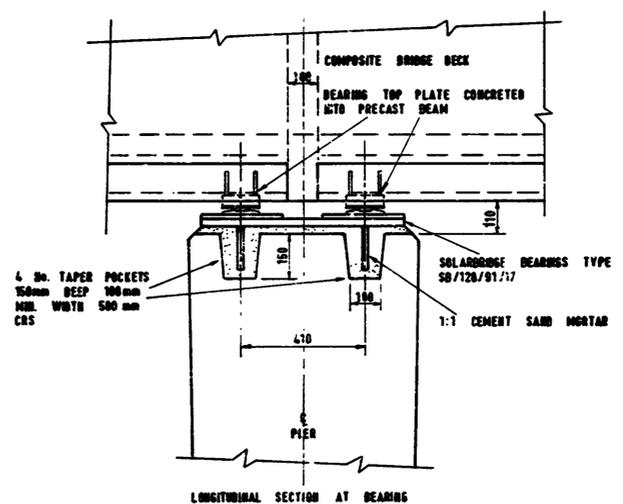
D-D SCALE 1:50



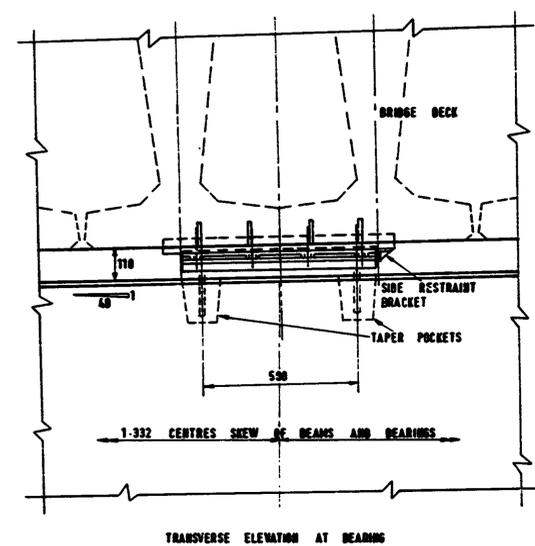
PLAN ON A-A SCALE 1:50

B-B SCALE 1:50

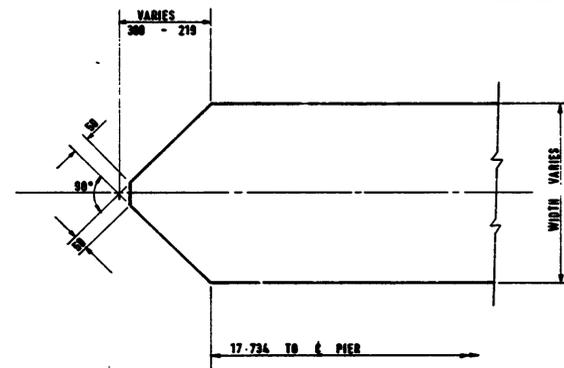
(PILE CAP OMITTED)



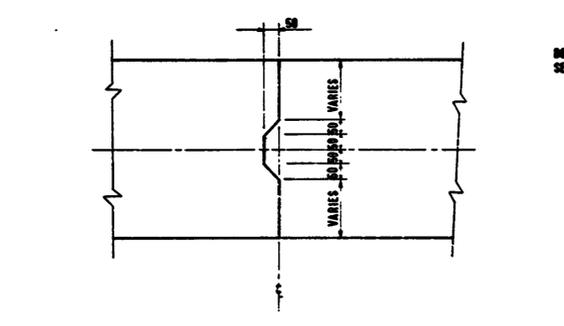
FREE PIER BEARING DETAILS



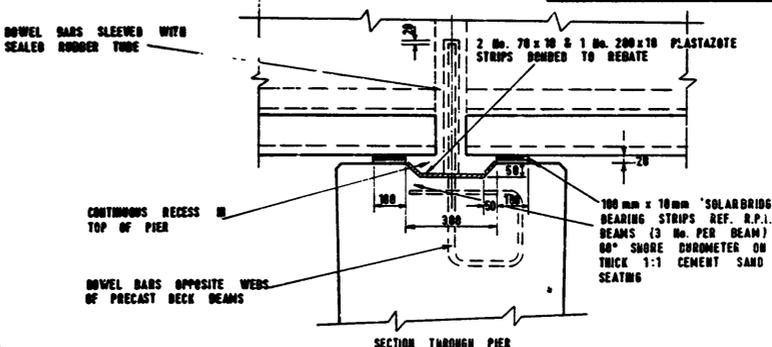
SCALE 1:10



PIER FACE DETAIL SCALE 1:10



DETAIL OF KEYED SHRINKAGE JOINT AT WALL



FIXED PIER BEARING DETAIL SCALE 1:10

NOTES

- FOR SETTING OUT DETAILS FOR BRIDGE, PILING ETC. SEE DRG. No. 405/201/011/3/E
- DETAILS OF REINFORCEMENT FOR PIERS IS SHOWN ON DRG. No. 405/201/011/0/0
- ALL CONCRETE FINISHES AS NOTED ON THE DRAWING
- ALL CONCRETE IN PIERS AND FOOTINGS CLASS 40/20
- WALLS OF PIERS CONCRETED IN FULL-HEIGHT LIFTS WITH THE EXCEPTION THAT A "KICKER" (MAXIMUM HEIGHT FROM TOP OF FOOTING 500) WAS PERMITTED AT THE BASE OF THE PIERS
- VERTICAL CONSTRUCTION JOINTS ARE KEYED JOINTS SIMILAR TO THE SHRINKAGE JOINT AT PIER CENTRE LINE
- FOR FULL DETAILS OF BEARINGS SEE MANUFACTURERS DRAWINGS

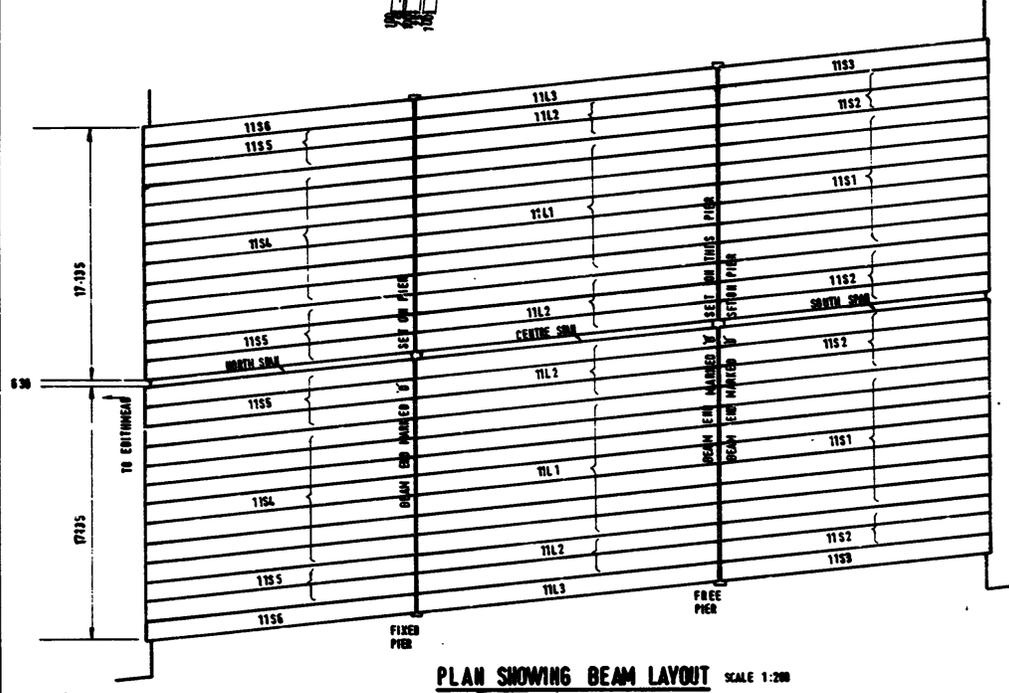
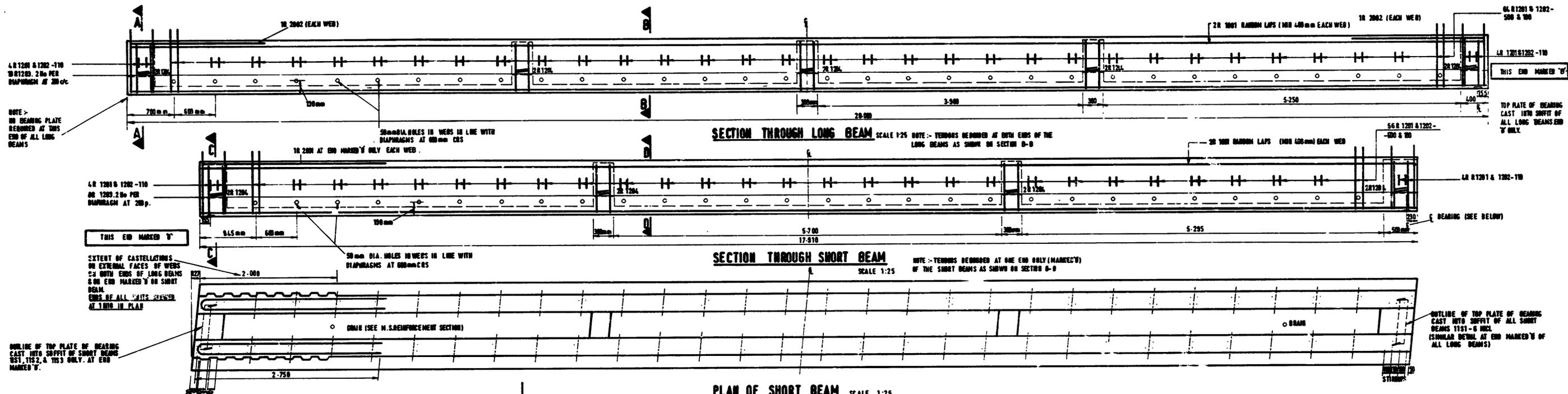
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng. C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

B	REBORN - "AS-BUILT" RECORD DRAWING	J.N.H.	4.9.74
A	DIMENSION 430 CORRECTED ON SECTION C-C		22.3.71
LET	AMENDMENT DETAILS	BY	DATE

BIRMINGHAM - EXETER MOTORWAY M5
SECTION : EDITHMEAD TO DUNBALL
RIVER BRUE BRIDGE DETAILS OF CENTRAL PIERS

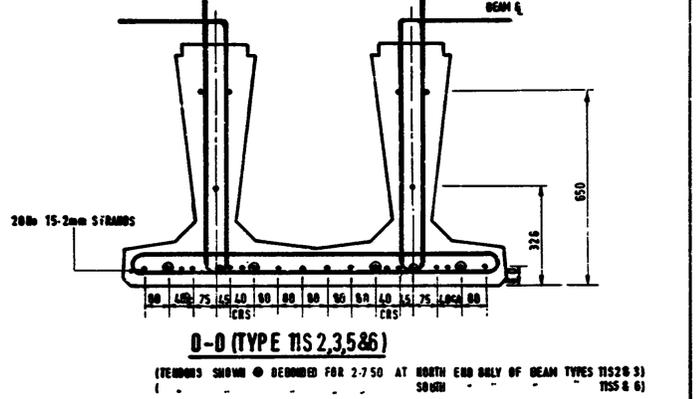
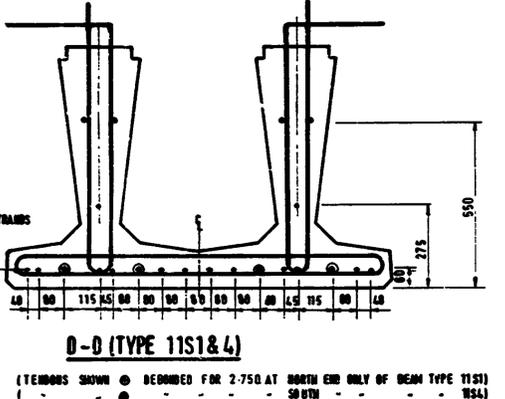
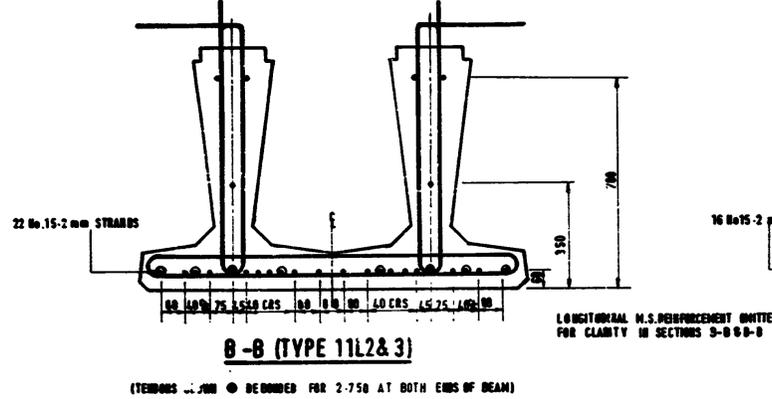
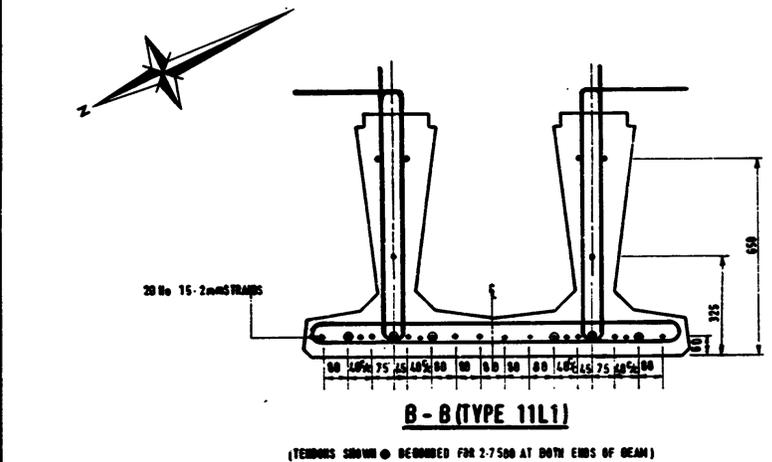
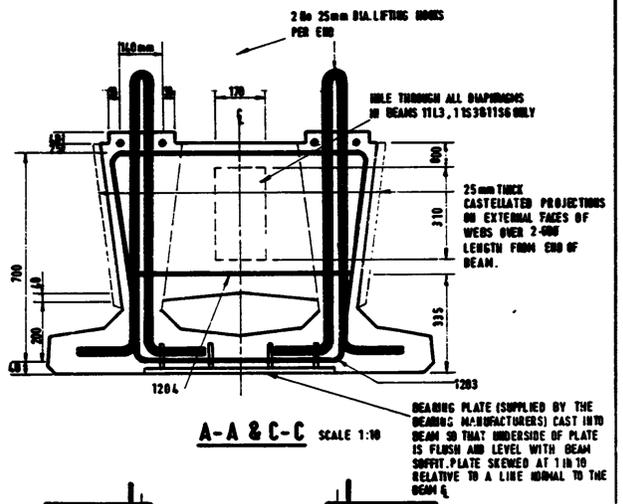
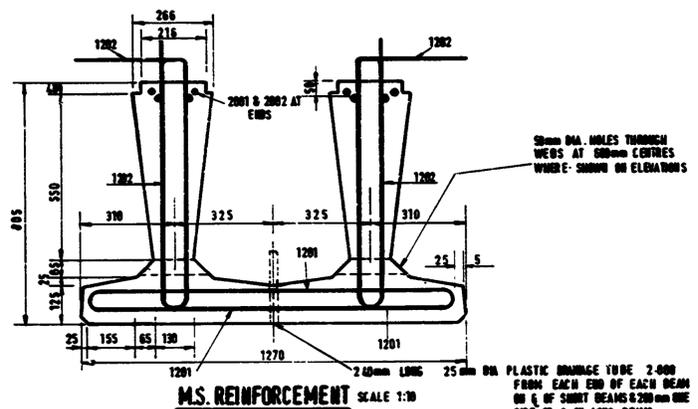
DRAWN C.M.H.	DATE	22.4.75	SCALES 1:50 1:10
	DATE	10.7.70	
	CHECKED A.J.P.	DATE	DRAWING No. 405/201/011/7/B



- NOTES**
1. CONCRETE CLASS 70/90/3/0
 2. STRENGTH AT TRANSFER $(28 \text{ days}) (600 \text{ p.s.f.})$
 3. STRANDS 15-2mm DIA. SHERMET STABILIZED STRANDS TO THE APPROVAL OF THE ENGINEER.
 4. EACH STRAND STRESSED TO 150 KN
 5. STRANDS INDICATED THIS \odot BENDED OVER THE END 2.750 AT BOTH ENDS OF BEAMS T1L2&3 AND AT ONE END ONLY OF BEAMS T1S1, T1S2, T1S3, T1S4, T1S5 & T1S6. BENDING EFFECTED BY APPROVED P.Y.E. SLEEVES OR STRANDS. ENDS MARKED 'Y' SET ON PIERS. ENDS OF LONG BEAMS MARKED 'Y' SET ON FREE PIER. ALL BEAMS CLEARLY AND UNDOUBTLY MARKED WITH THE BEAM REFERENCE MARK.
 6. LIFTING HOOKS CAST IN WEBS AT END DIAPHRAGMS ONLY. HOOKS CUT OFF AFTER ERECTION IN ACCORDANCE WITH ENGINEERS REINFORCEMENTS. BEAMS LIFTED ONLY BY HOOKS PROVIDED.
 7. AN ALLOWANCE OF 45mm HAS BEEN MADE FOR JOINTS BETWEEN ADJACENT UNITS.
 8. ENDS OF ALL BEAMS LEFT UNTOUCHED.
 9. TOLERANCES TO CLAUSE 1713 M.C.T. SPECIFICATION
 10. FIRST TWO NUMBERS IN M.S. REINFORCEMENT REFERENCES DENOTE DIA. IN MILLIMETERS.

SCHEDULE OF PRETENSIONED BEAMS

ID OFF	MARK	LENGTH ALONG ξ	LOCATION
14	T1S1	17-910	SOUTH SPAN
10	T1S2	17-910	
2	T1S3	17-910	CENTRE SPAN
14	T1L1	20-000	
10	T1L2	20-000	NORTH SPAN
2	T1L3	20-000	
14	T1S4	17-910	NORTH SPAN
10	T1S5	17-910	
2	T1S6	17-910	SPAN

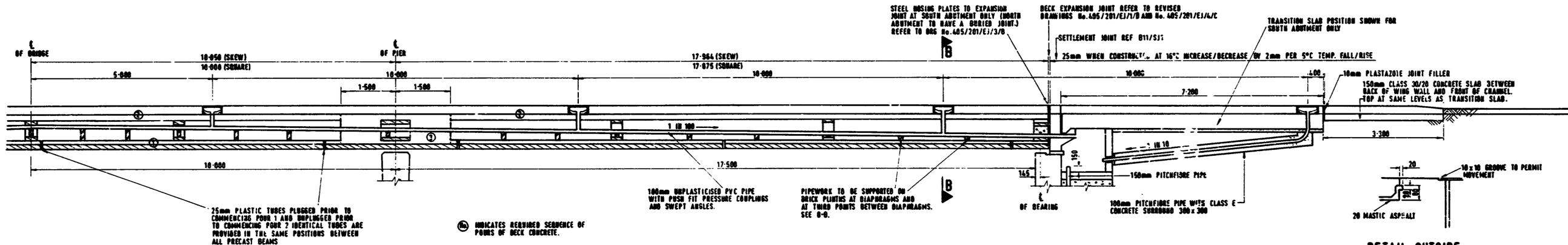


DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

C	REVISION 'AS-BUILT' RECORD DRAWING		
B	EACH WEB NOTE ADDED, T202&2001 REFERENCED ON M.S. SECTION		
A	DIM FOR BEARING ξ AT ABUTMENT AMENDED		
LET	AMENDMENT DETAILS	BY	DATE

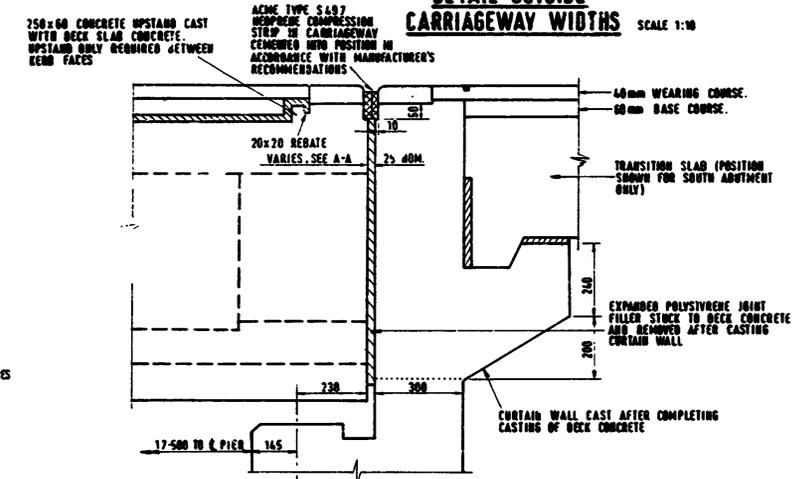
BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
RIVER BRUE BRIDGE P/S CONCRETE BEAM DETAILS

SHEET No AS-BUILT DRAWING	DRAWN P.E.	DATE 3-78	SCALES 1:25, 1:20, 1:10 & 1:200
	TRACED B.R.	DATE 12.11.78	
	CHECKED A.J.P.	DATE 10.7.78	
	DRAWING No 405/201/B11/9/C		

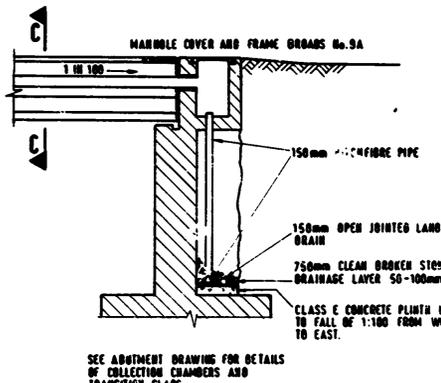


A-A SCALE 1:50

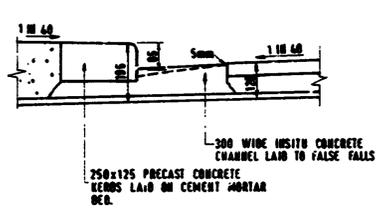
DETAIL OUTSIDE CARRIAGEWAY WIDTHS SCALE 1:10



E-E SCALE 1:10



D-D SCALE 1:50



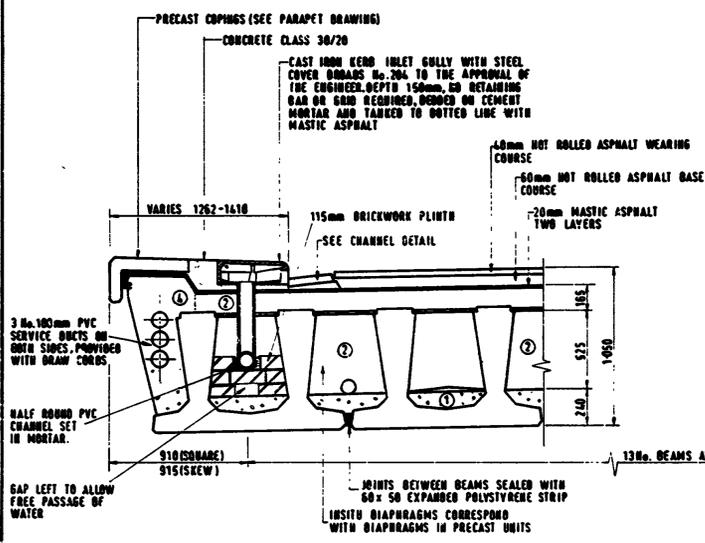
CHANNEL DETAIL SCALE 1:10

DETAIL OF FALSE FALLS

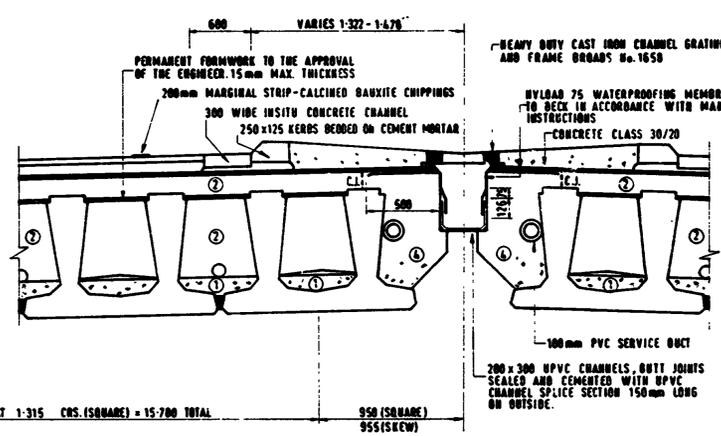
SCALES: VERT. 1:5, HORIZ. 1:50
THE SAME BASIC ARRANGEMENT APPLIES ON EITHER SIDE OF EXPANSION JOINT BUT HIGH POINT IS TOP OF STEEL WISINGS AND BURIED JOINT POSITION.

DECK INSITU TOPPING LEVELS			
POSITION	R.L. 'A'	R.L. 'B'	
North Abutment Wall	6-926	7-336	
Fixed Pier	6-919	7-329	
Free Pier	6-916	7-326	
South Abutment Wall	6-908	7-318	

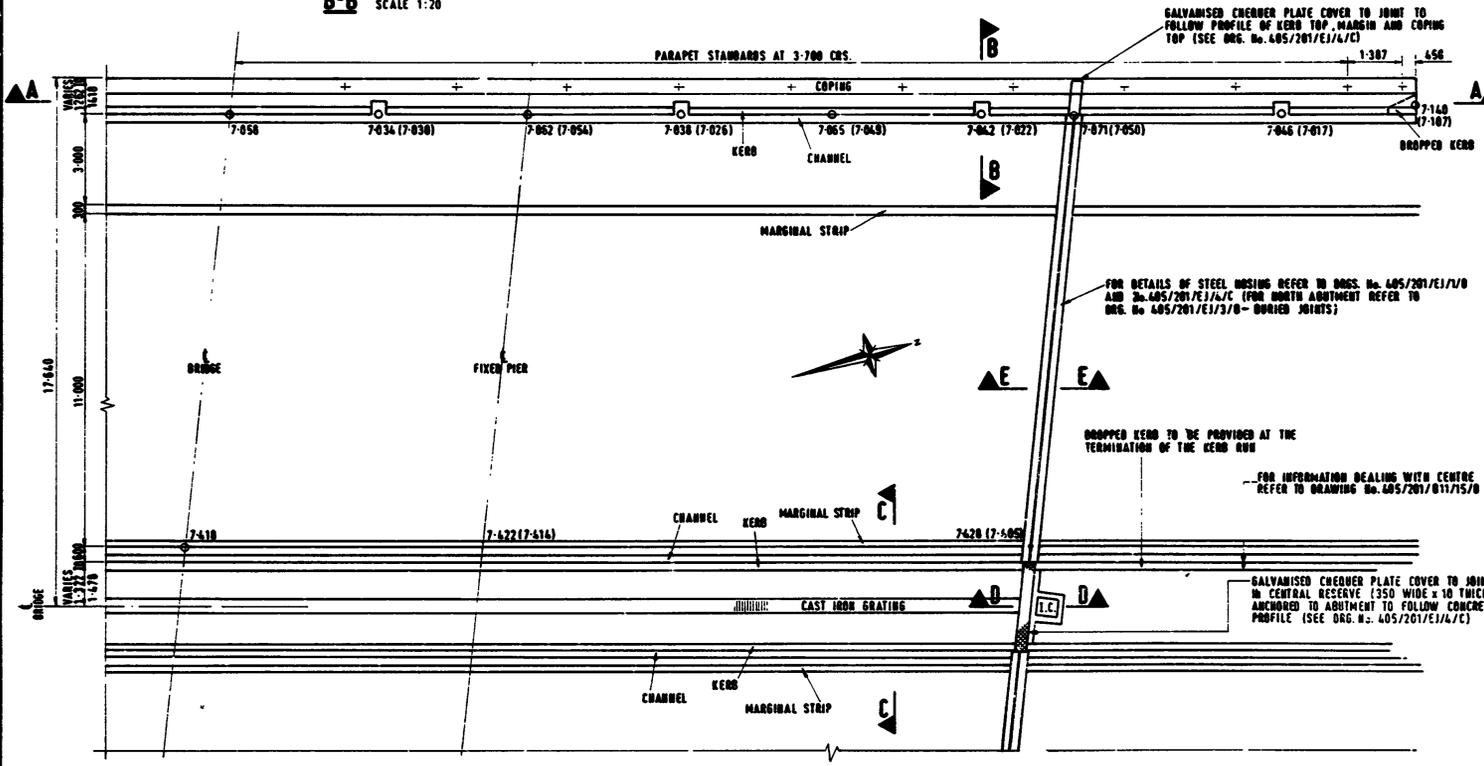
- NOTES
- STRUCTURAL DECK CONCRETE CLASS 40/20.
 - DECK CONCRETE WAS CAST IN THE SEQUENCE SHOWN.
 - SPOT LEVELS INDICATED THIS WAY (7-421) LEVELS IN BRACKETS eg (7-017) APPLY TO SOUTHERN HALF OF DECK.
 - FOR DETAILS OF VERTICAL AND HORIZONTAL CURVATURE OF MOTORWAY ALIGNMENT IN RELATION TO BRIDGE DECK PROFILE SEE DRG. No. 405/201/1/1/A.
 - FOR INFORMATION RELATING TO SOUTH EXPANSION JOINT SEE DRGS. No. 405/201/E1/1/D AND E1/1/E.



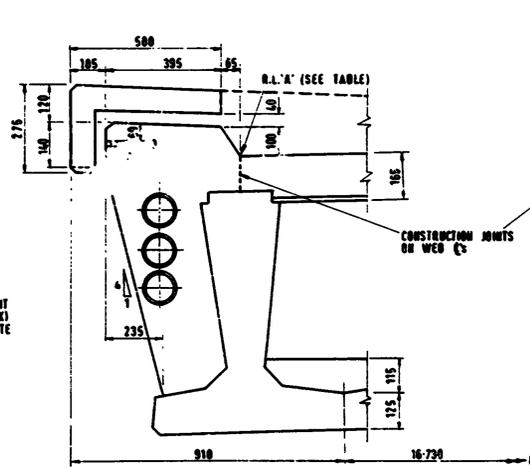
B-B SCALE 1:20



C-C SCALE 1:20



PART PLAN SCALE 1:100



EDGE BEAM DETAIL SCALE 1:10

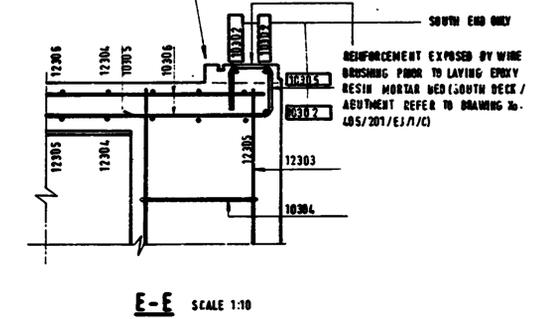
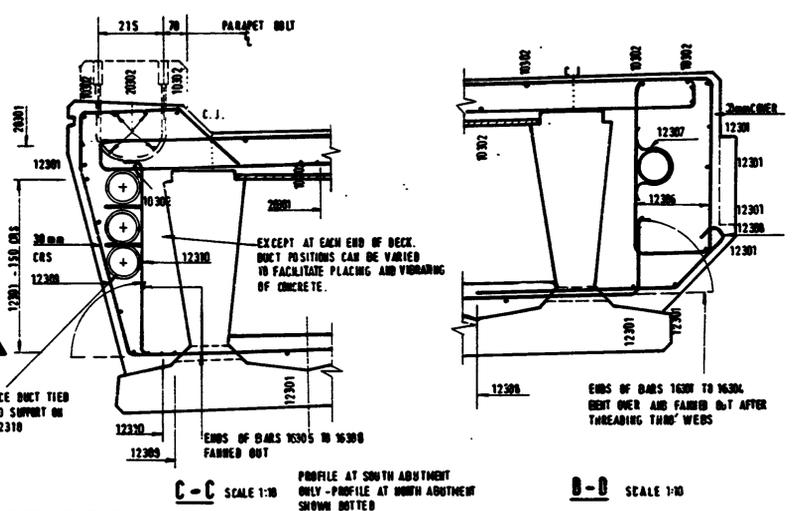
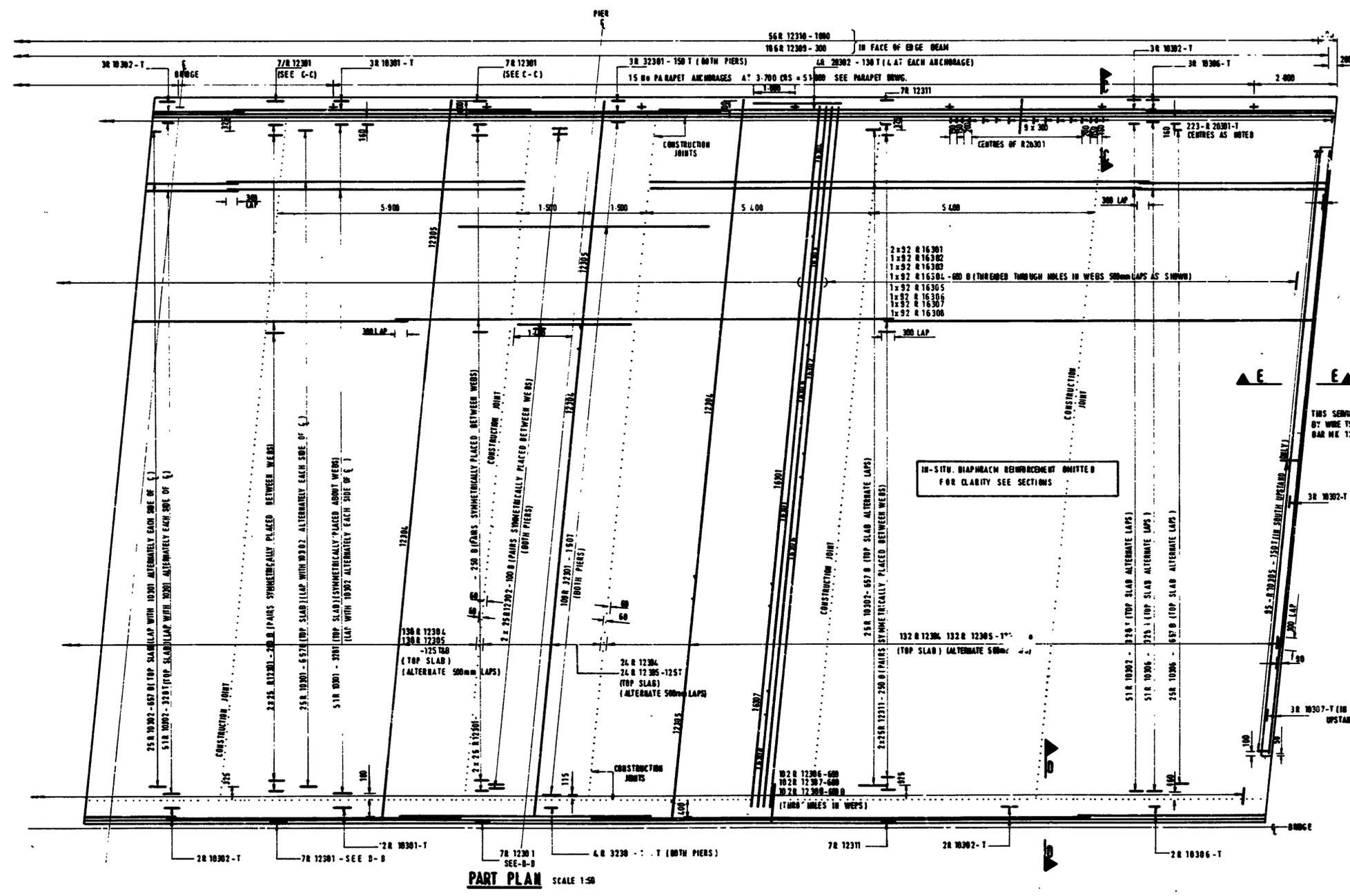
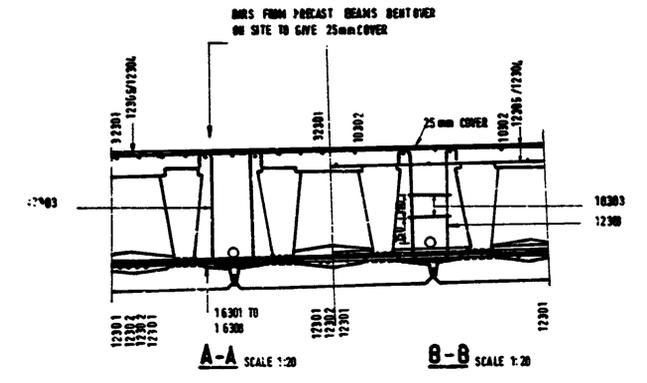
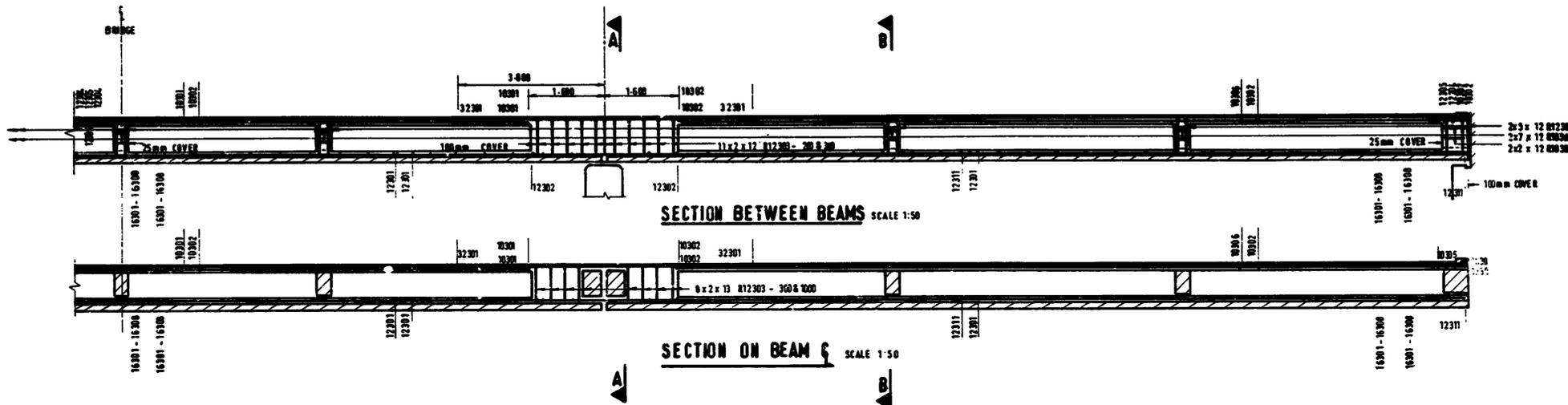
DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
Director and Engineer P. G. Lyth B.Eng. C.Eng.
SOMERSET COUNTY COUNCIL SUB-UNIT

NO.	DESCRIPTION	BY	DATE
B	REBRANW-AS-BUILT RECORD DRAWING	J.M.H	SEPT '76
C	REVISED EXP. JNT. INFO. ADDED & POSITION OF KERBS IN CENTRE RESERVE	P.W.	JUNE '77
C	AMENDED. TRANSITION SLAB REPOSITIONED AT NORTH ABUTMENT	P.W.	JAN '77
B	SECTION A-A LENGTHS OF SPANS & END PARAPET BAY DIMS. AMENDED	P.W.	JAN '77
A	SECTION A-A LENGTH OF SIDESPAN AMENDED	P.W.	JAN '77
LET	AMENDMENT DETAILS	BY	DATE

BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
RIVER BRUE BRIDGE

DECK LAYOUT

SHEET No.	DRAWN P.W.	DATE FEB '70	SCALES 1:100, 1:20, 1:50, 1:5
"AS BUILT" DRAWING	TRACED M.V.	DATE MARCH '77	1:10
CHECKED A.J.P.	DATE JULY '70	DRAWING No. 405/201/B11/10/D	



- NOTES
- NUMBERS OF BARS ARE SHOWN FOR ONE DECK ONLY.
 - COVER TO REINFORCEMENT 30mm UNLESS NOTED OTHERWISE.

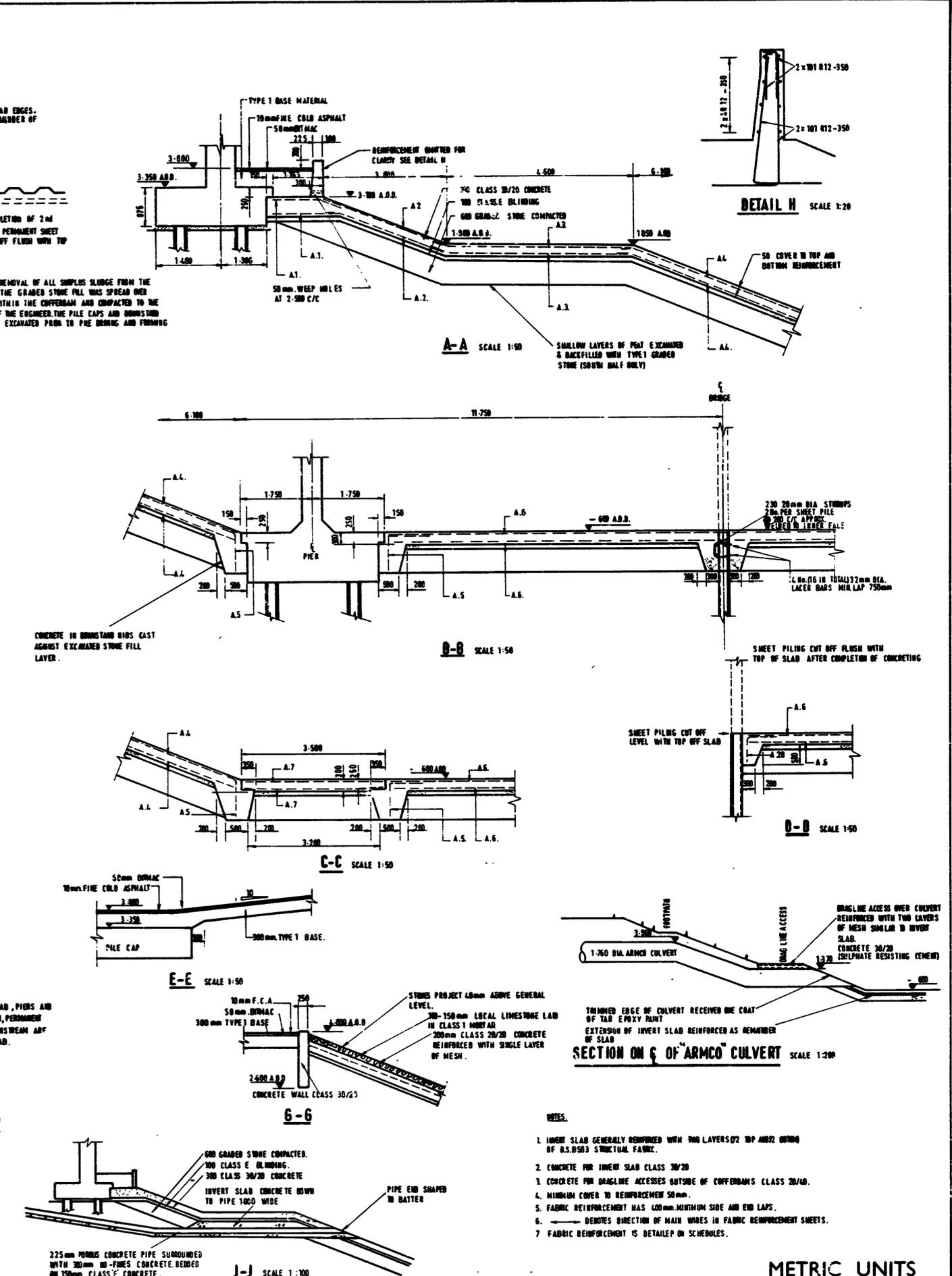
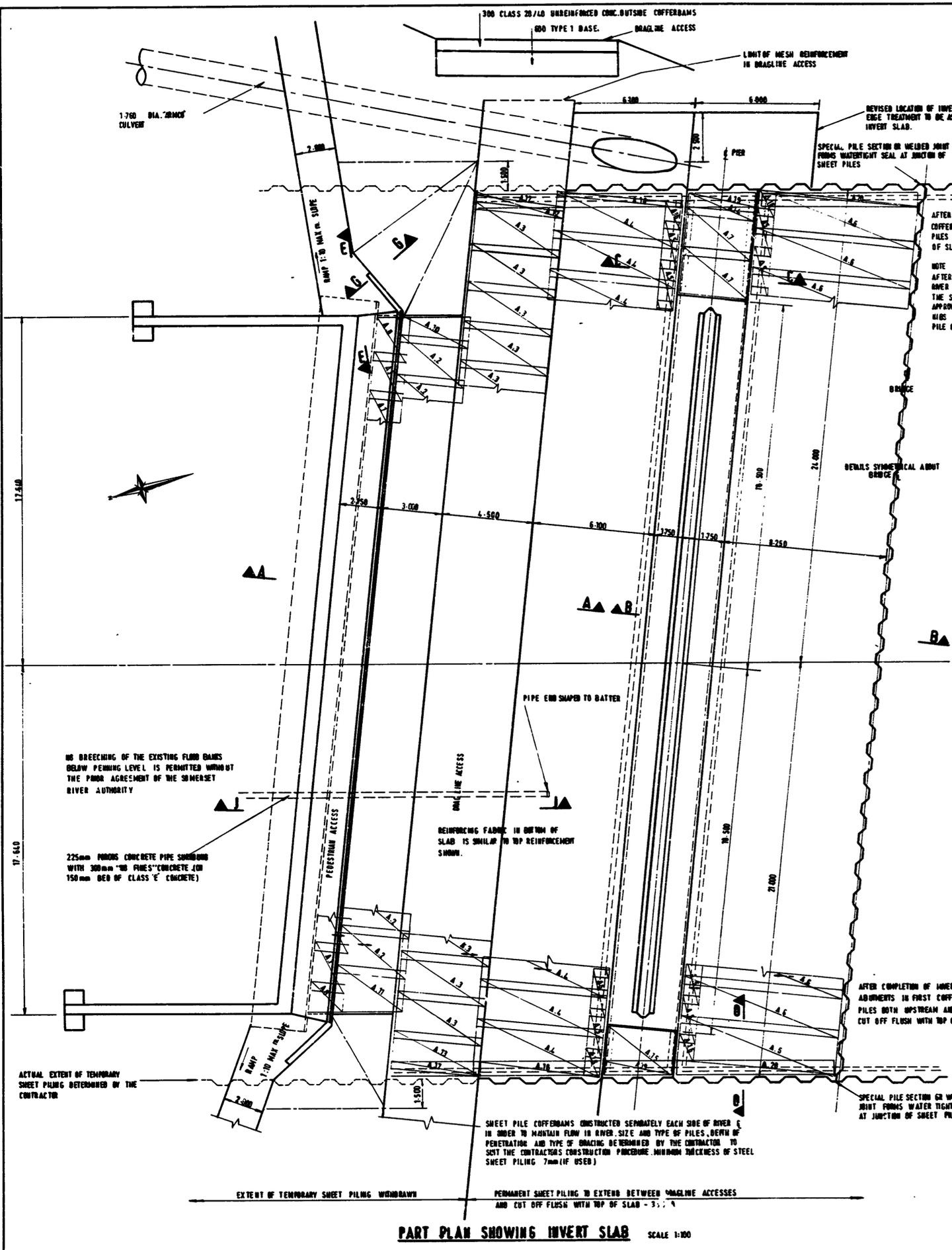
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

B	REDRAWN "AS-BUILT" RECORD DRAWING	J.L.R.	4.9.74
C	REVISED EXPANSION JOINT INFORMATION ADDED	E.J.A.	5.2.73
D	STAYPUT DIMENSIONS AMENDED ON C-C.	P.W.	11.1.72
A	134 R 12304 AND 12305 REQUIRED NOT 160	E.B.	19.3.71
LET	AMENDMENT DETAILS	BY	DATE

BIRMINGHAM - EXETER MOTORWAY M5
 SECTION: EDITHMEAD TO DUNBALL
 RIVER BRUE BRIDGE DECK REINFORCEMENT

SHEET No	DRAWN	DATE	SCALES
"AS-BUILT" DRAWING	TRACED B.A.R.	DATE 27/10/76	1:10 & 1:50
	CHECKED A.J.P.	DATE 10.7.70	DRAWING No 4/05/201/811/11/0



- NOTES:
1. INVERT SLAB GENERALLY REINFORCED WITH TWO LAYERS OF 225 TOP AND 200mm OF B.S.5053 STRUCTURAL FABRIC.
 2. CONCRETE FOR INVERT SLAB CLASS 30/20
 3. CONCRETE FOR DRAGLINE ACCESSES OUTSIDE OF COFFERDAMS CLASS 20/40.
 4. MINIMUM COVER TO REINFORCEMENT 50mm.
 5. FABRIC REINFORCEMENT HAS 400mm MINIMUM SIDE AND END LAPS.
 6. ——— DEMOTES DIRECTION OF MAIN WIRES IN FABRIC REINFORCEMENT SHEETS.
 7. FABRIC REINFORCEMENT IS DETAILED IN SCHEDULES.

PART PLAN SHOWING INVERT SLAB SCALE 1:100

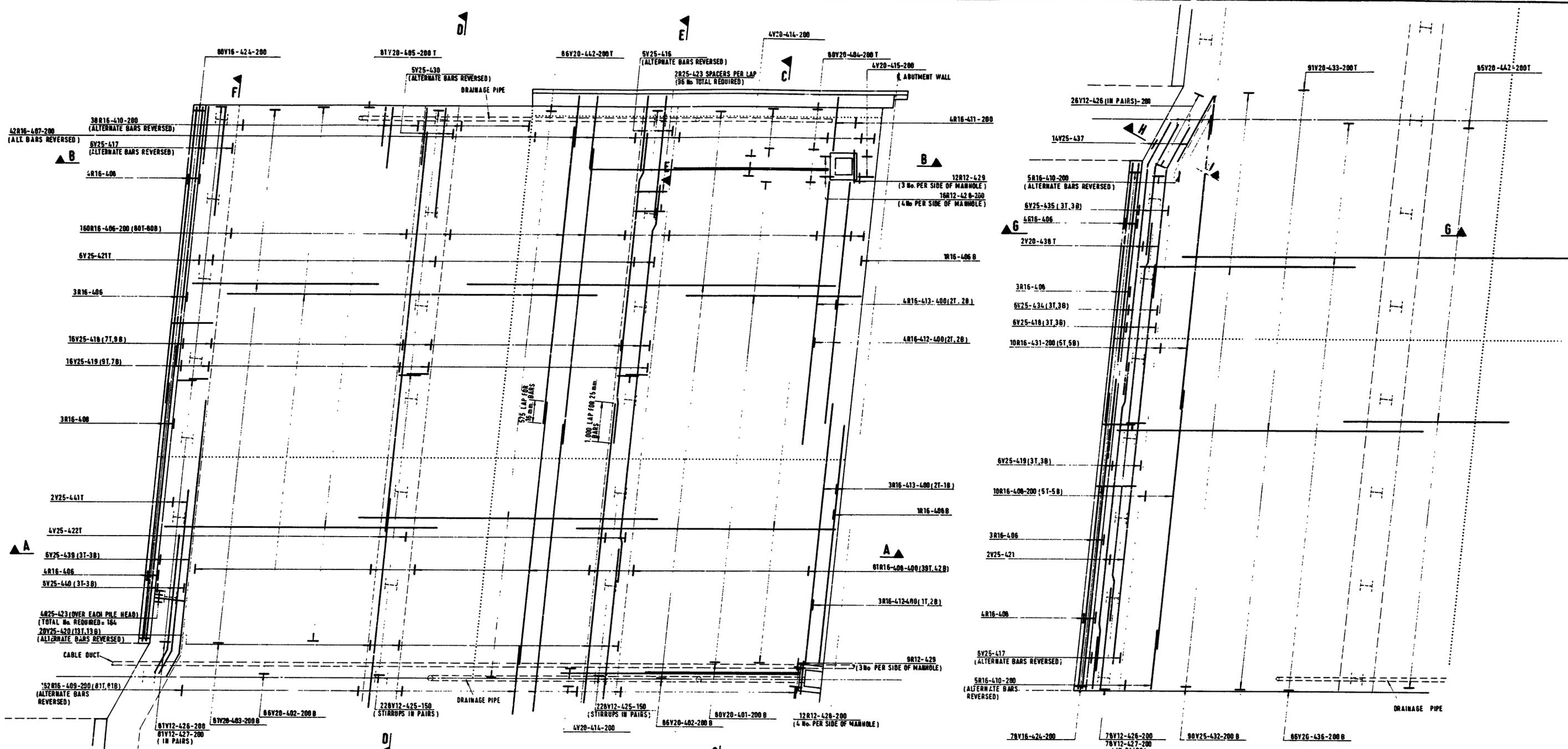
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

8	REBORN AS-BUILT RECORD DRAWING	J.H.R.	4.9.74
4	DEPTH OF PILE CAPS INCREASED IN A-A AND B-B	P.W.	1.9.72
LET	AMENDMENT DETAILS	BY	DATE

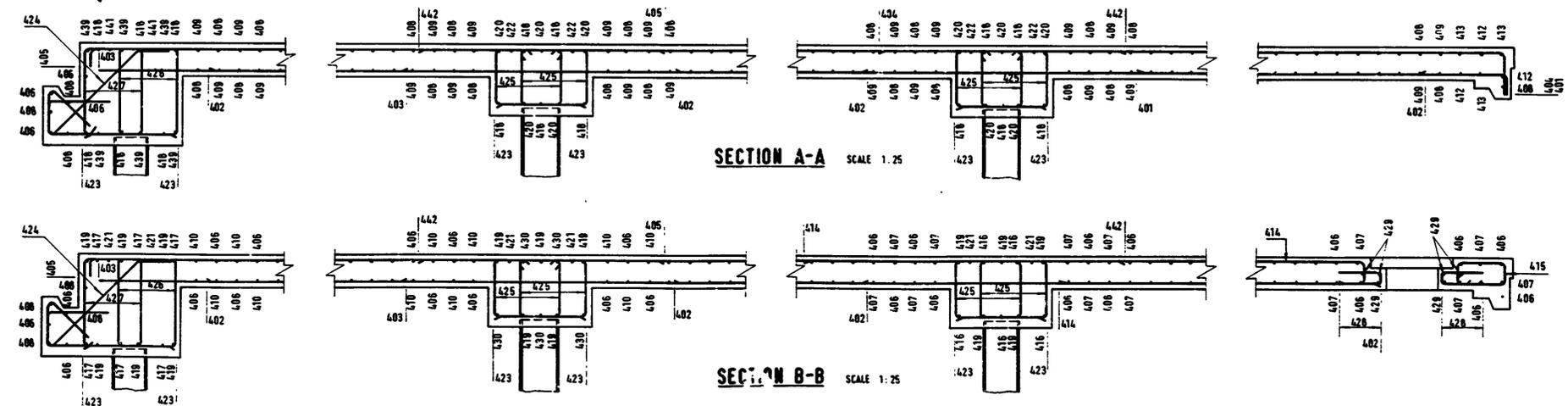
BIRMINGHAM - EXETER MOTORWAY M 5
SECTION : EDITHMEAD TO DUNBALL
RIVER BRUE BRIDGE DETAILS OF INVERT SLAB TO RIVER BED AND BANKS

SHEET No	DRAWN	DATE	SCALES
AS-BUILT DRAWING	E.M.D.	21.5.70	1:20, 1:50, 1:100, 1:200
	TRACED	DATE	
	B.A.R.	21.10.76	
	CHECKED	DATE	DRAWING No
			405/201/811/14/8



HALF PLAN OF APPROACH SPAN [EAST SIDE]
SCALE 1:50

PART PLAN OF APPROACH SPAN [WEST SIDE] SCALE 1:50



- NOTES**
- 1 CONCRETE COVER : BOTTOM STEEL 45 mm, TOP STEEL 35 mm.
 - 2 THIS DRAWING READ IN CONJUNCTION WITH DRG No. 405/201/811/17/A
 - 3 FOR CONSTRUCTIONAL DETAILS OF APPROACH SPANS SEE DRAWING No. 405/201/811/15/D
 - 4 REINFORCEMENT BARS SHOWN ON PLAN OF EAST SIDE ARE SYMMETRICAL ABOUT LONGITUDINAL & UNLESS SHOWN OTHERWISE ON PART PLAN OF WEST SIDE. No. OF BARS SHOWN GENERALLY APPLIES TO THE HALF PLAN ONLY.
 - 5 REINFORCEMENT ADJUSTED LOCALLY TO ACCOMMODATE DRAINAGE OUTLET PIPES.

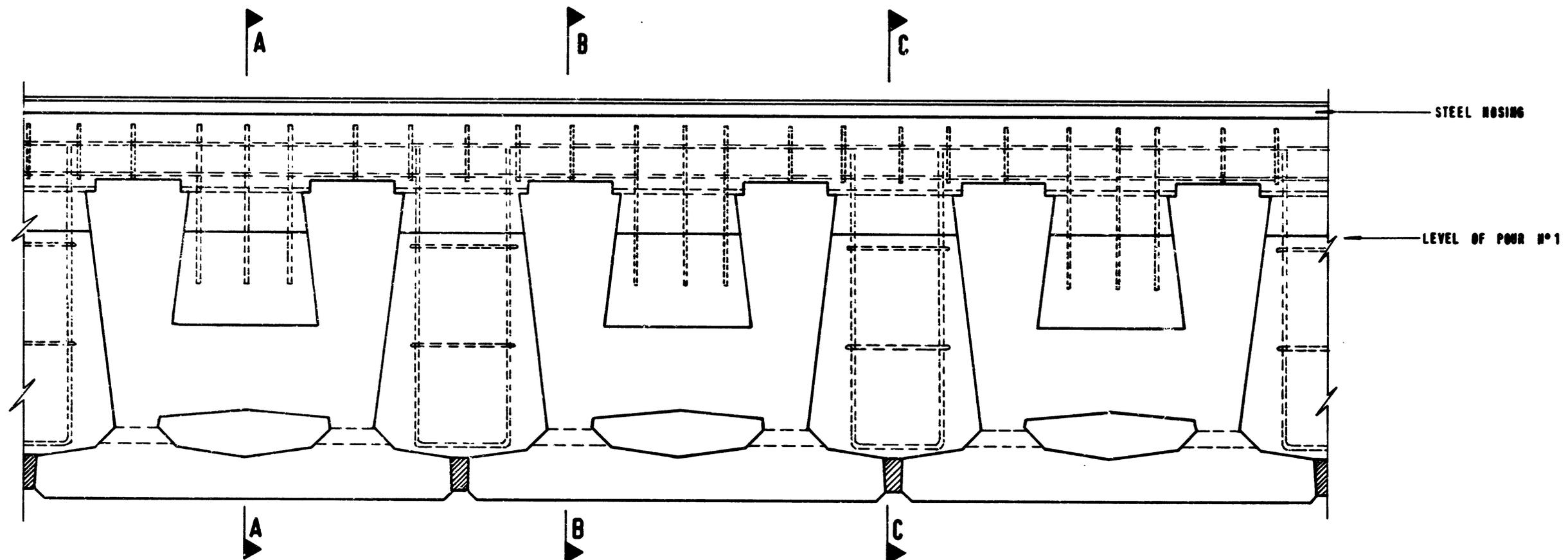
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT, S.W.R.C.U.
Director and Engineer P. G. Lyth B.Eng., C.Eng.
SOMERSET COUNTY COUNCIL SUB-UNIT

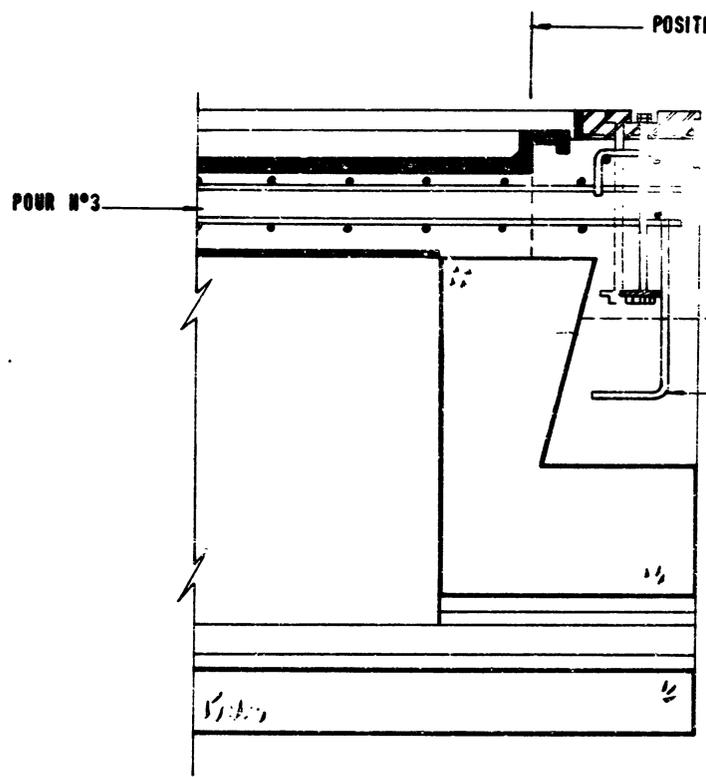
RE-DRAWN - "AS-BUILT" RECORD DRAWING	BY	DATE
AMENDMENT DETAILS		

BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
RIVER BRUE BRIDGE APPROACH SPAN REINFORCEMENT 1

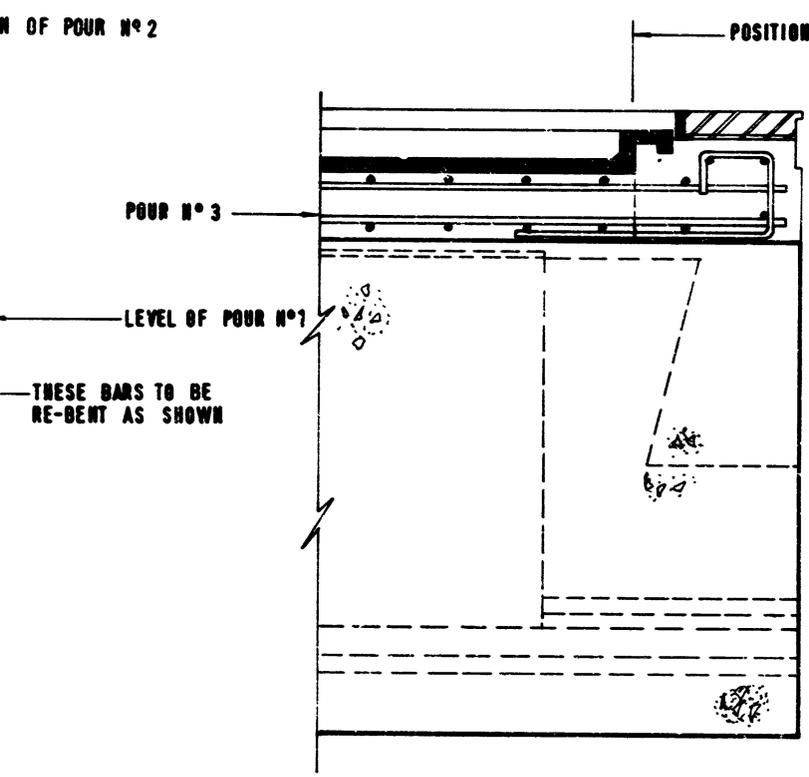
SHEET No	DRAWN P.W.	DATE 31.5.72	SCALES 1:50 1:25
"AS-BUILT" DRAWING	TRACED J.C.	DATE 19.6.72	
CHECKED J.P.T.	DATE JULY 72	DRAWING No 405/201/811/16/A	



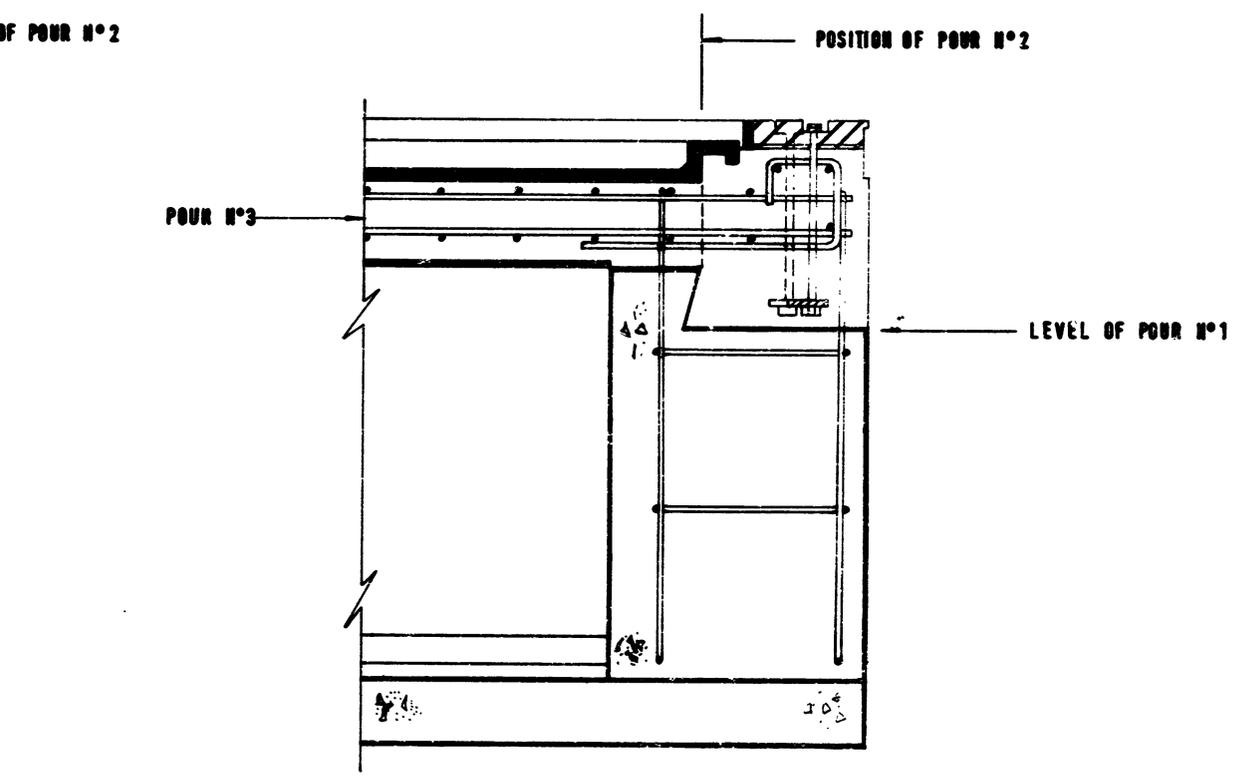
VIEW AT END OF BEAMS



SECTION A-A



SECTION B-B



SECTION C-C

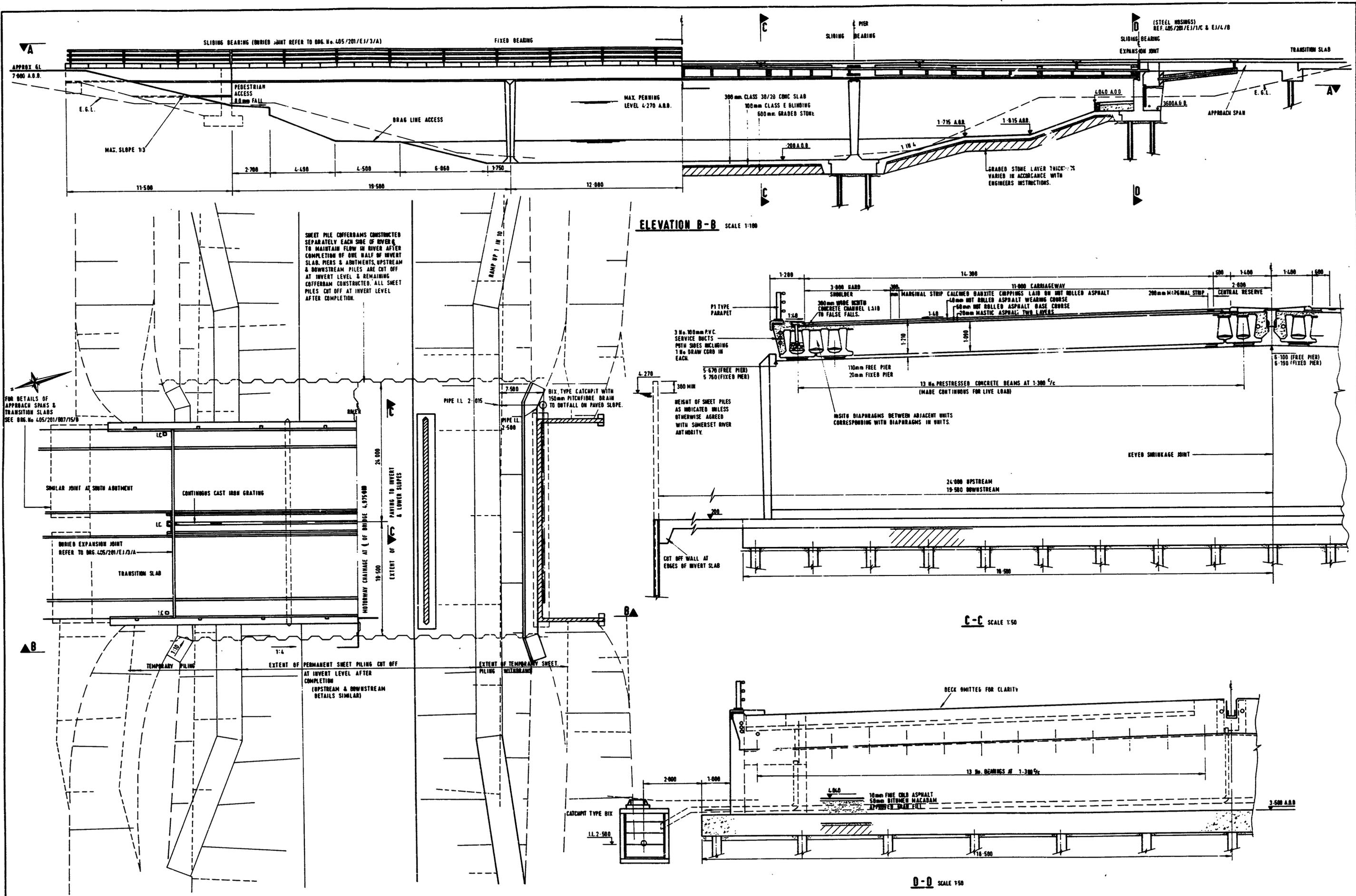
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT SOUTH WESTERN ROAD CONSTRUCTION UNIT
 Director and Engineer P G Lyth B. Eng., C. Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT.

LET	AMENDMENT DETAILS	BY	DATE

RIVER BRUE & HUNTSPIILL AMENDMENTS TO REINFORCEMENT & SEQUENCE OF PLACING CONCRETE AT EXPANSION JOINTS

DRAWN	DATE	SCALES
J.R.	DEC 72	1:10
TRACED	DATE	DRAWING NO.
S.M.	JAN 75	405 / 201
CHECKED	DATE	



FOR DETAILS OF APPROACH SPANS & TRANSITION SLABS SEE DRG. No. 405/201/807/15/8

DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

PLAN ON A-A SCALE 1:250

ELEVATION B-B SCALE 1:100

C-C SCALE 1:50

D-D SCALE 1:50

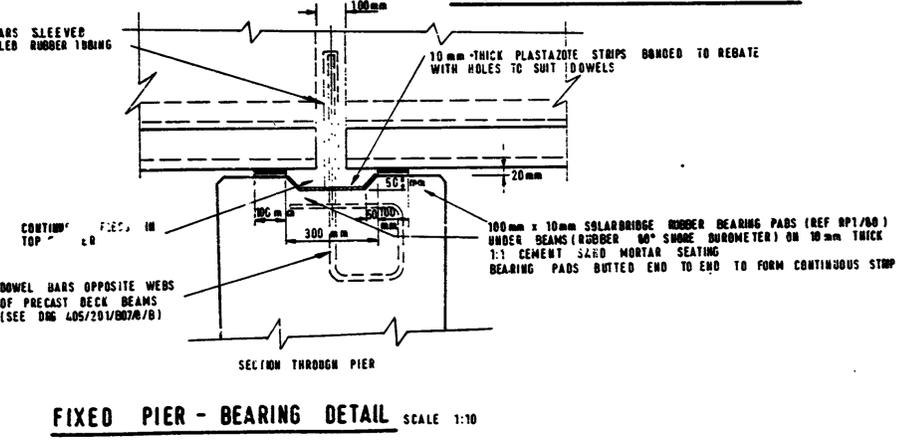
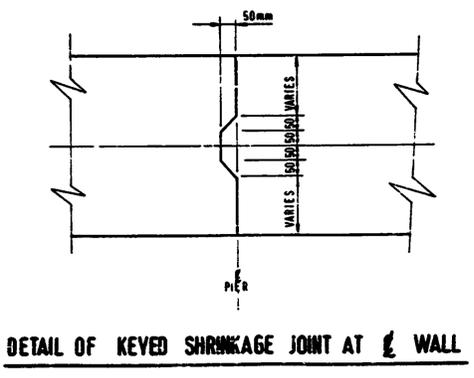
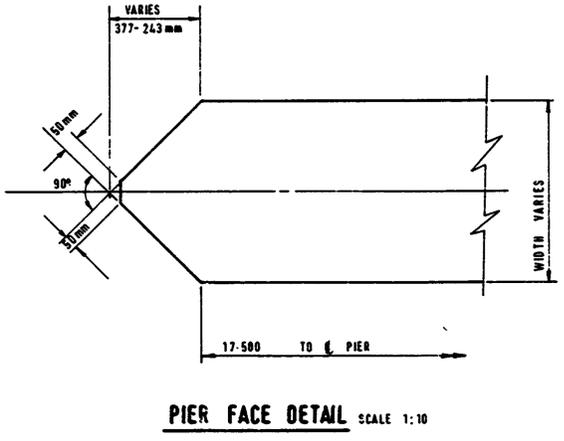
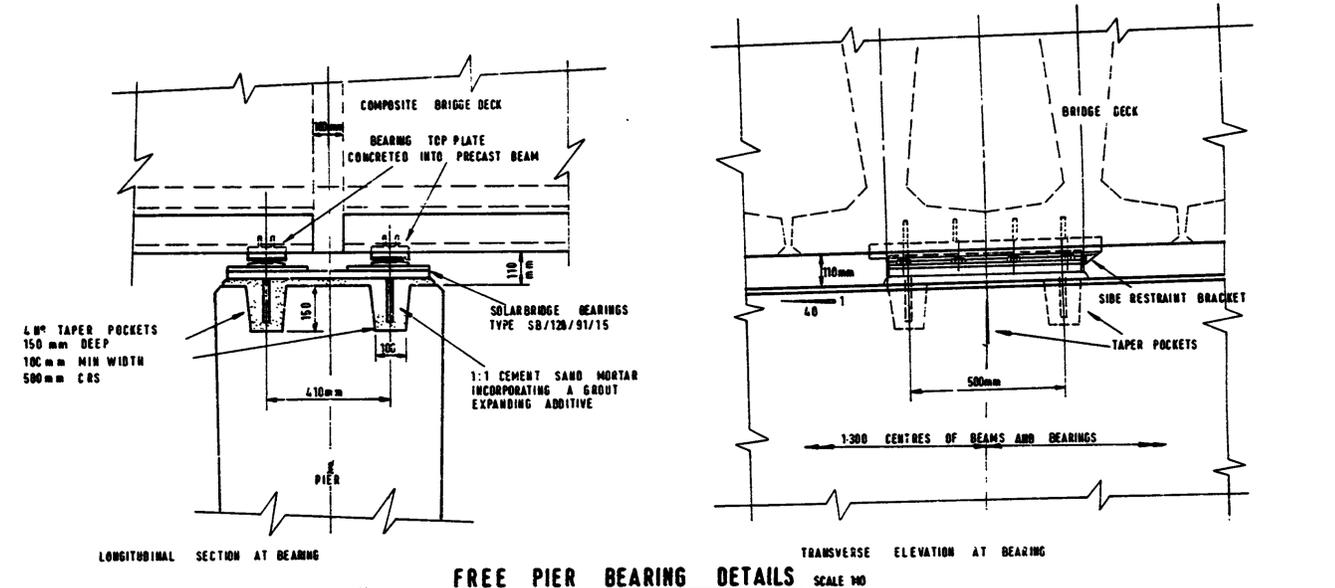
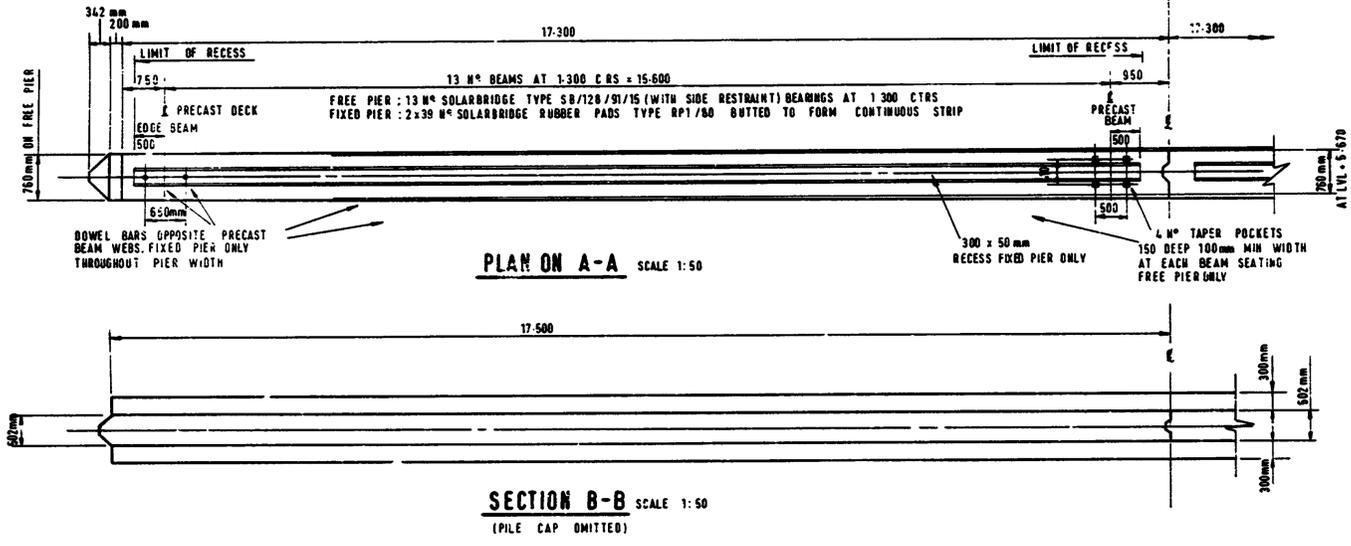
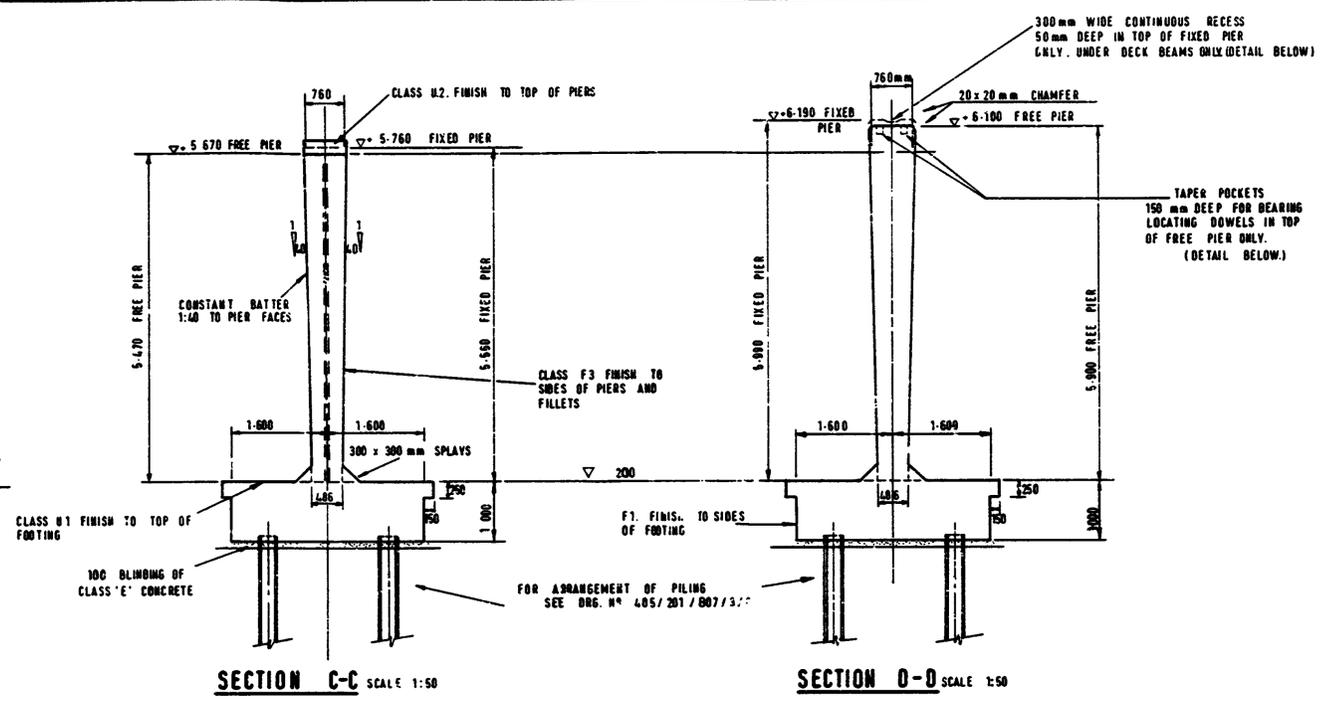
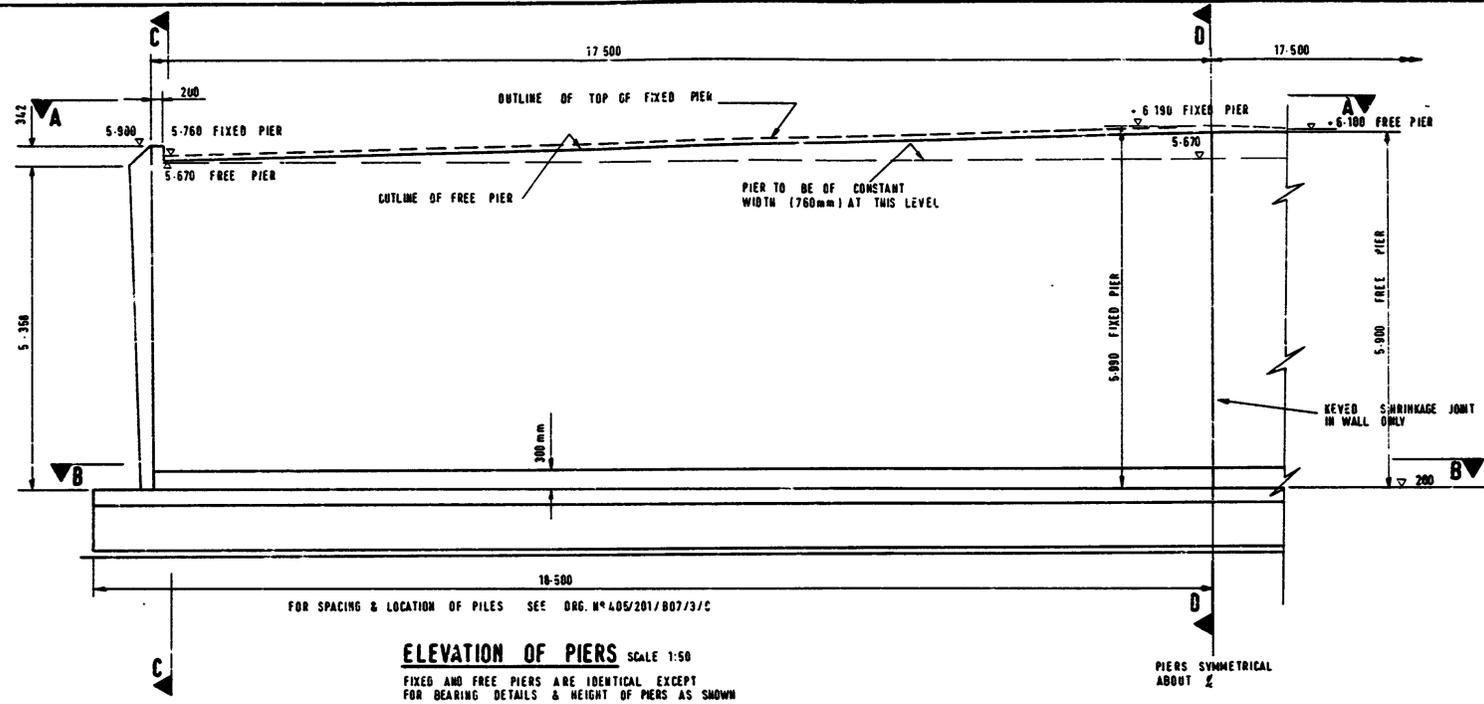
METRIC UNITS

LET	AMENDMENT DETAILS	BY	DATE
D	RE-DRAWN AS-BUILT DRAWING	J.H.R.	23 8 74
C	REVISED EXPANSION JOINT INFORMATION & APPROACH SPANS ADDED	E.J.R.	FEB. 73
B	DRAG LINE ACCESS LEVELS AMENDED	P.W.	30 5 72
A	END PARAPET BAY AMENDED	P.W.	5 1 72

BIRMINGHAM-EXETER MOTORWAY M5
SECTION : EDITHMEAD TO DUNBALL
HUNTSPILL RIVER BRIDGE

GENERAL ARRANGEMENT

SHEET No	DRAWN	DATE	SCALES
'AS-BUILT' DRAWING	P.R.G.	DATE	1:250 1:100 1:50
	TRACED	DATE	
	M.S.	SEPT 75	
	CHECKED	DATE	DRAWING No
	A.J.P.	7-7-78	405/201/807/2/D



- NOTES**
- FOR SETTING OUT DETAILS FOR BRIDGE, PILING ETC. SEE DRG. N° 405/201/801/3/C
 - DETAILS OF REINFORCEMENT FOR PIERS IS SHOWN ON DRG. N° 405/201/801/6/B
 - ALL CONCRETE FINISHES AS NOTED ON THE DRAWING.
 - ALL CONCRETE IN PIERS AND FOOTINGS CLASS 40/20
 - WALLS OF PIERS CONCRETED IN FULL HEIGHT LIFTS WITH THE EXCEPTION THAT A 'KICKER' (MAXIMUM HEIGHT FROM TOP OF FOOTING 500) WILL BE PERMITTED AT THE BASE OF THE PIERS
 - ANY VERTICAL CONSTRUCTION JOINTS ARE KEVED JOINTS SIMILAR TO THE SHRINKAGE JOINT AT PIER CENTRE LINE
 - FOR FULL DETAILS OF BEARINGS SEE MANUFACTURERS DRAWINGS.

METRIC UNITS

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 SOMERSET COUNTY COUNCIL SUB-UNIT

B	REDRAWN "AS-BUILT" RECORD DRAWING	JRN	23.8.73
A	PILE CAP DIMENSIONS ADDED		
LET	AMENDMENT DETAILS	BY	DATE

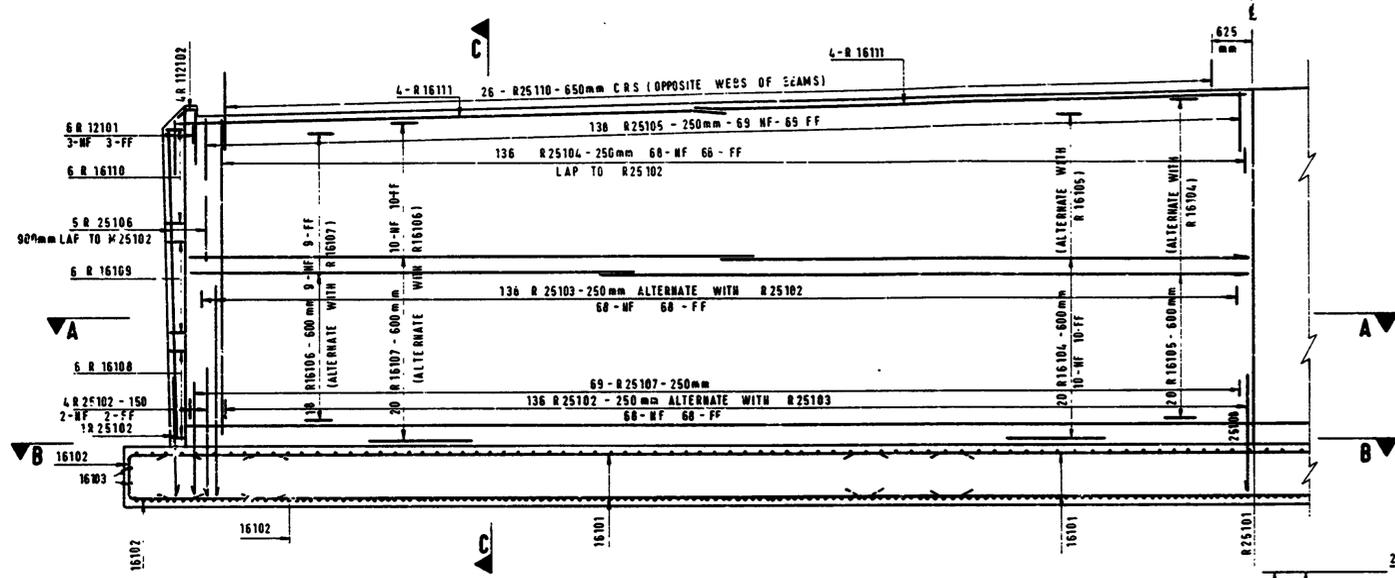
BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
HUNTSPILL RIVER BRIDGE

DETAILS OF PIERS

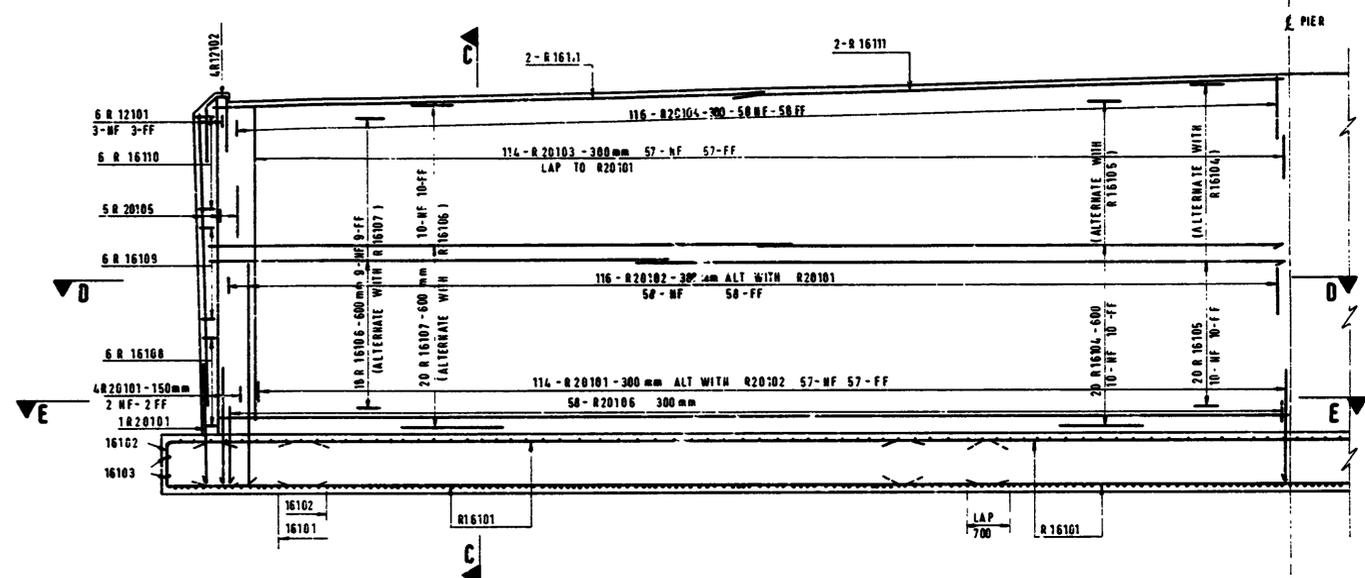
DRAWN L.B.	DATE	SCALES
	1.8	1:50, 1:10
	TRACED S.P.	DATE SEPT '75
CHECKED A.J.P.	DATE 7.70	DRAWING No 405/201/807/7/B

FIXED PIER

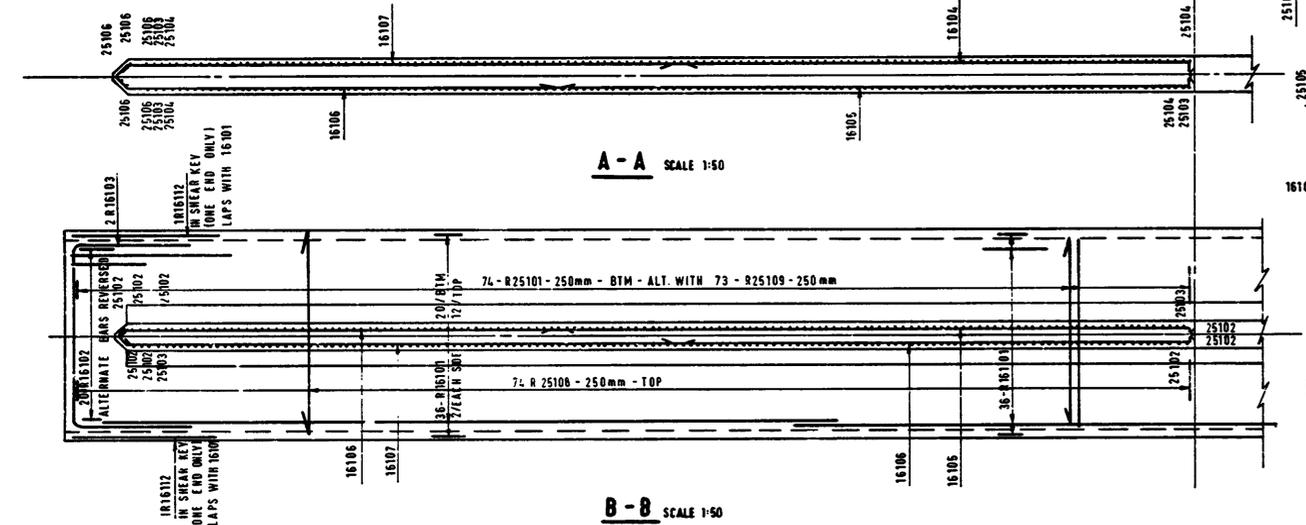
FREE PIER



PIER ELEVATION (PART) SCALE 1:50

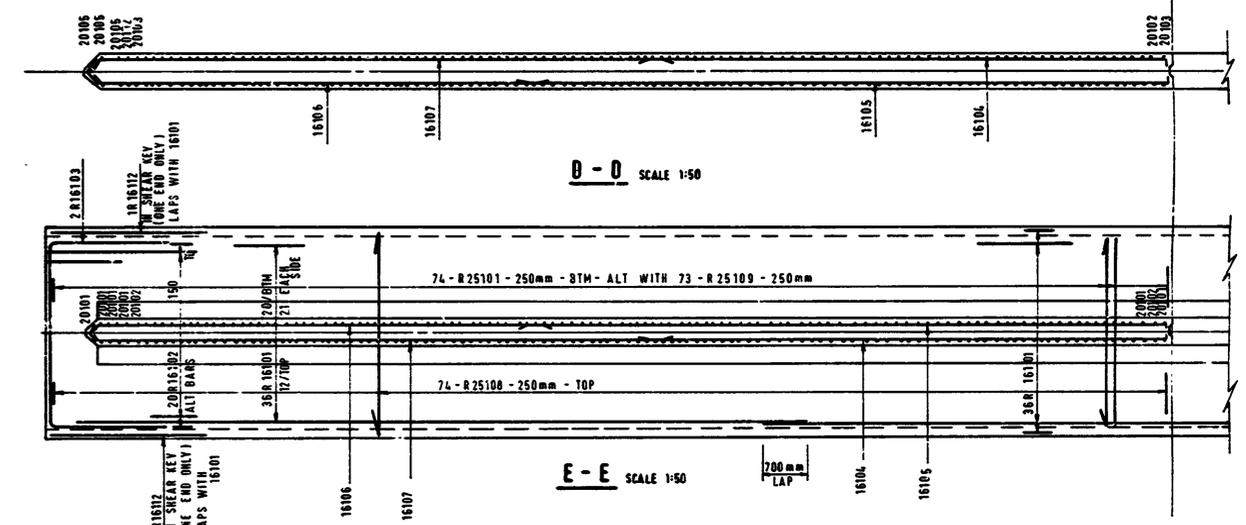


PIER ELEVATION (PART) SCALE 1:50



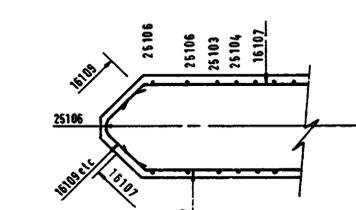
A - A SCALE 1:50

B - B SCALE 1:50

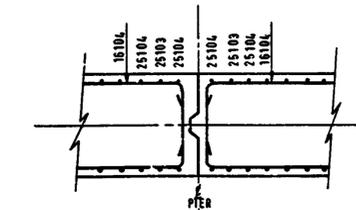


D - D SCALE 1:50

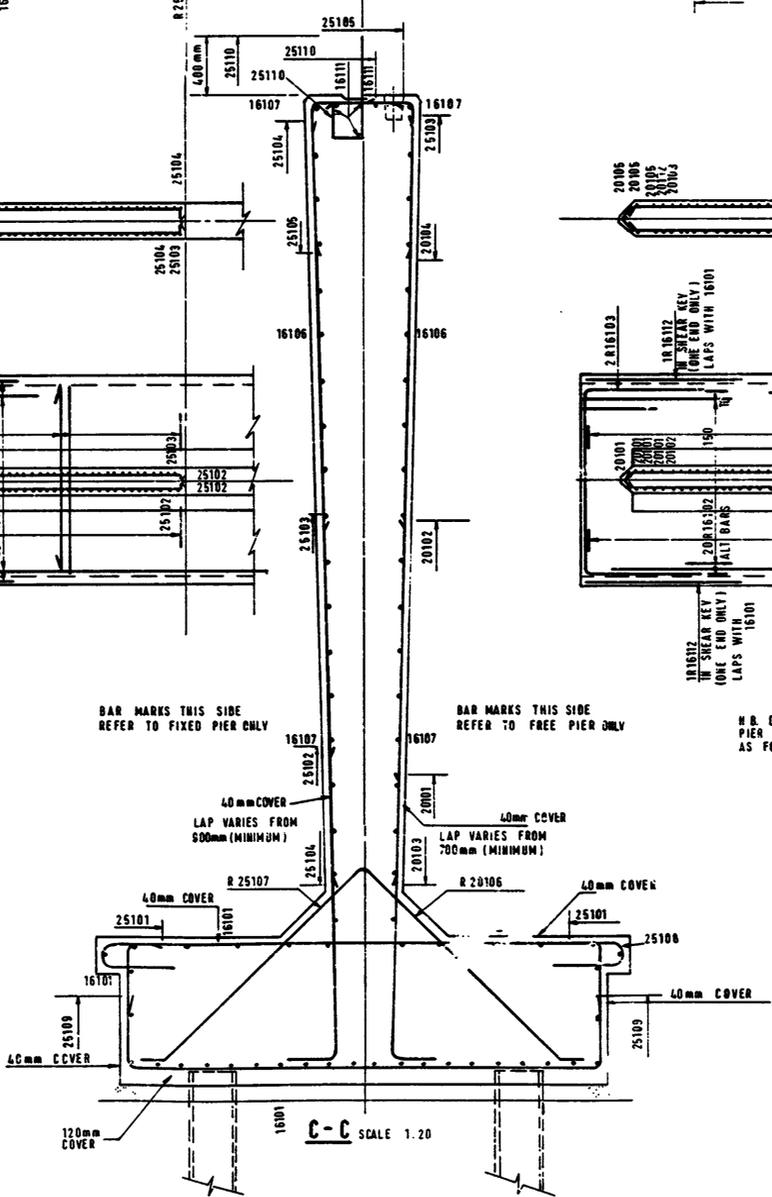
E - E SCALE 1:50



ENLARGED EDGE DETAIL (A-A) SCALE 1:20



ENLARGED DETAIL AT CENTRAL JOINT (A-A) SCALE 1:20



C - C SCALE 1:20

- NOTES**
1. FOR OUTLINE DETAILS OF PIERS SEE DRG. N° 405/201/807/1/B
 2. MIN. COVER TO REINFORCEMENT 40mm
 3. REINFORCEMENT IN PIERS SYMMETRICAL ABOUT \bar{x}

METRIC UNITS

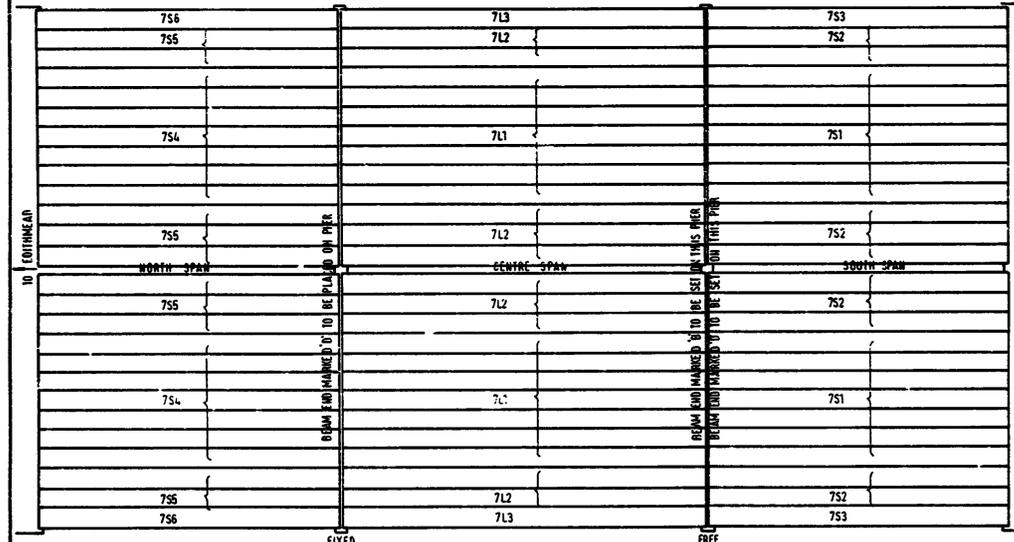
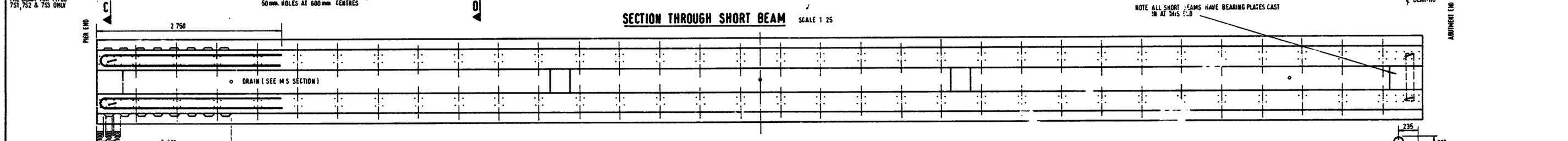
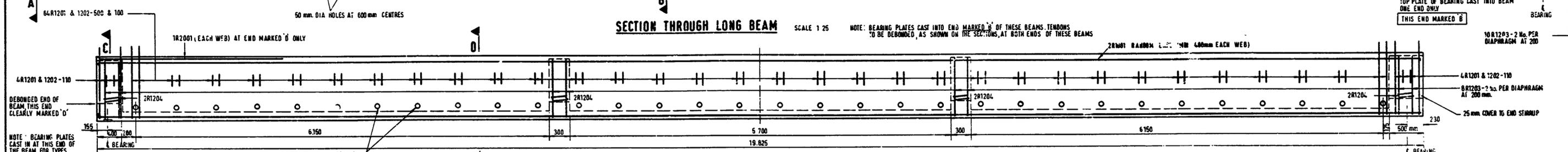
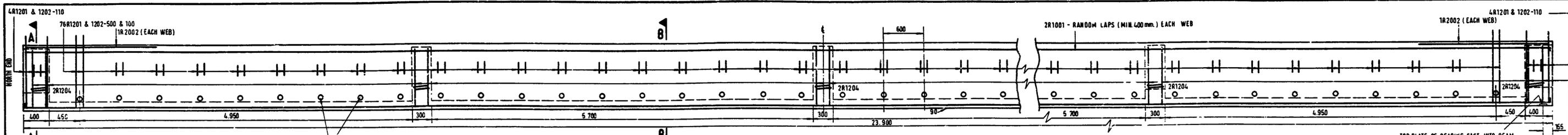
DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

RE DRAWN	'AS-BUILT' RECORD DRAWING	J.N.R.	23.8.74
A	R 25103 N° REQUIRED INCREASED		
LET	AMENDMENT DETAILS	BY	DATE

BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
HUNTSPILL RIVER BRIDGE

REINFORCEMENT FOR PIERS

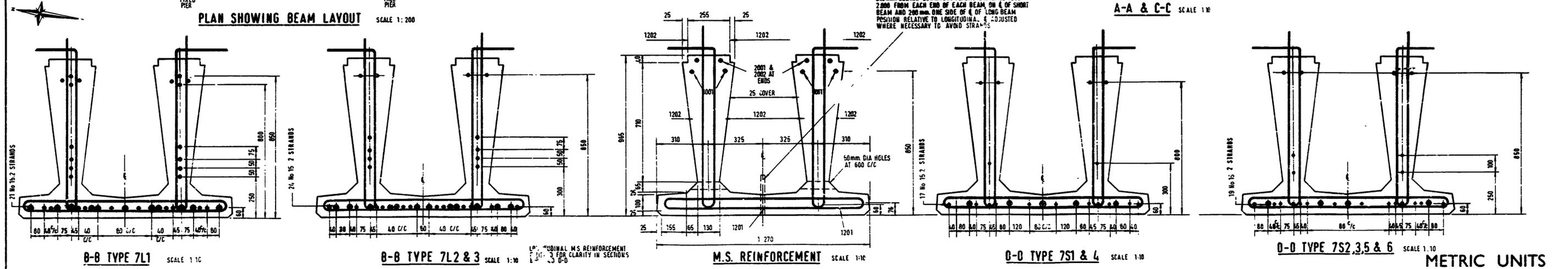
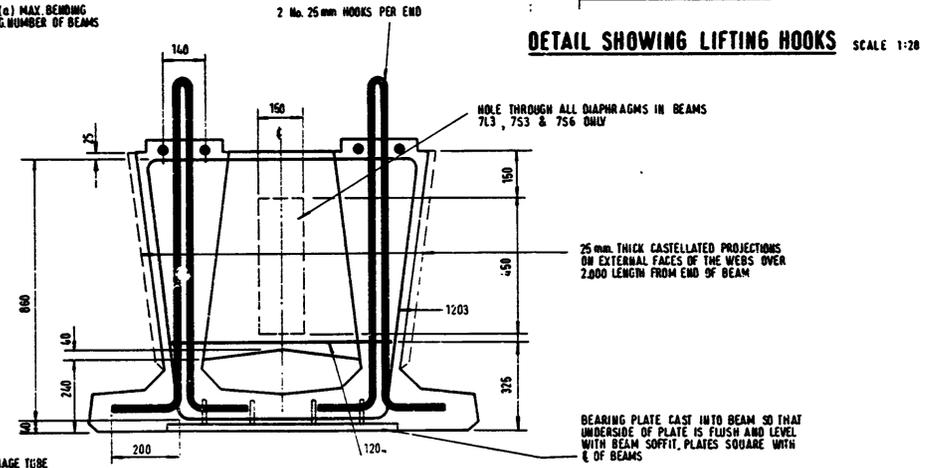
"AS-BUILT" DRAWING	DRAWN	K.B.	DATE	SCALES
	TRACED	S.J.	DATE	1:20 1:50
	CHECKED	A.J.P.	DATE	DRAWING No
			7.70	405/201/807/8/B



- NOTES**
1. CONCRETE CLASS 50/10
 2. STRENGTH AT TRANSFER 42 N/mm²
 3. STRANDS 15.2 mm DIA. SOMERSET STABILISED STRANDS OR EQUIVALENT TO THE APPROVAL OF THE ENGINEER
 4. EACH STRAND STRESSED TO 159 kN
 5. STRANDS INDICATED THIS \odot DEBONDED OVER THE END 2.750 AT BOTH ENDS OF BEAMS 7L1, 2 & 3 AND AT ONE END ONLY OF BEAMS 7S1-6 INCL.
 6. DEBONDED ENDS OF SHORT BEAMS CLEARLY MARKED 'D' DEBONDING EFFECTED BY PVC SLEEVES OR STRAPS
 7. LIFTING HOOKS CAST IN WEBS AT END DIAPHRAGMS ONLY. HOOKS CUT OFF AFTER ERECTION IN ACCORDANCE WITH THE ENGINEERS REQUIREMENTS. BEAMS LIFTED ONLY BY THE HOOKS PROVIDED.
 8. AN ALLOWANCE OF 30mm HAS BEEN MADE FOR JOINTS BETWEEN ADJACENT UNITS
 9. WHERE THE ENGINEER SO DIRECTED THE BEAMS WERE TESTED BY APPLICATION OF LOAD AT THIRD POINTS LOAD WAS SUCH THAT (a) MAX BENDING STRESS \Rightarrow UT x 0.9 OR (b) MAX TENSILE STRESS \Rightarrow UT x 0.1 WHERE UT = CUBE STRENGTH AT TIME OF TEST WHICH EVER WAS THE LOWER LOADING NUMBER OF BEAMS TESTED = 4
 10. ENDS OF ALL BEAMS WERE LEFT UNTREATED
 11. TOLERANCES TO CLAUSE 7.15 M.C.T. SPECIFICATION
 12. FIRST TWO NUMBERS IN M.S. REINFORCEMENT BAR REFERENCES DENOTE DIA. IN MILLIMETRES

SCHEDULE OF PRETENSIONED BEAMS

No. OFF	MARK	LENGTH	LOCATION
14	7L1	23.900	CENTRE
10	7L2	-	SPAN
2	7L3	-	SPAN
14	7S1	19.825	SOUTH
10	7S2	-	SPAN
2	7S3	-	SPAN
14	7S4	19.825	NORTH
10	7S5	-	SPAN
2	7S6	-	SPAN



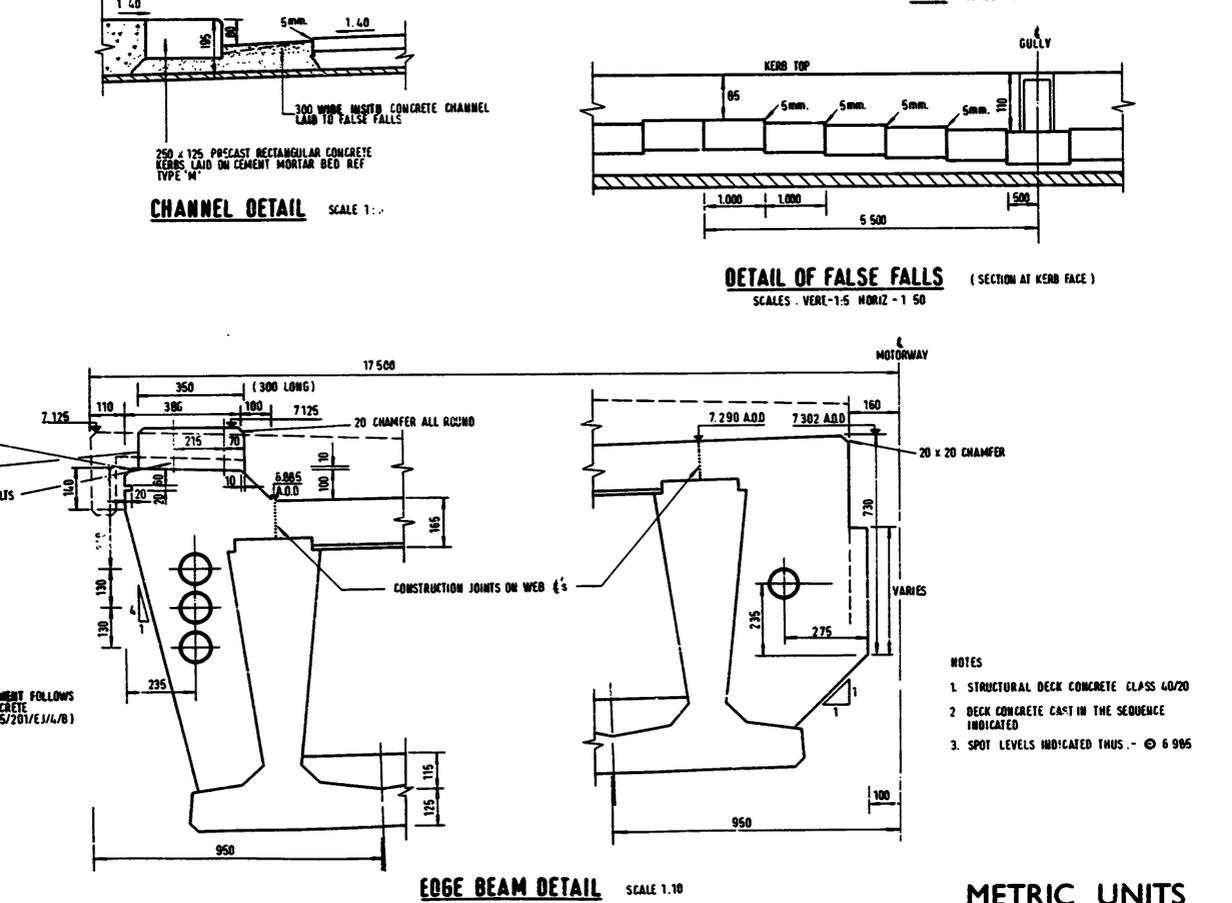
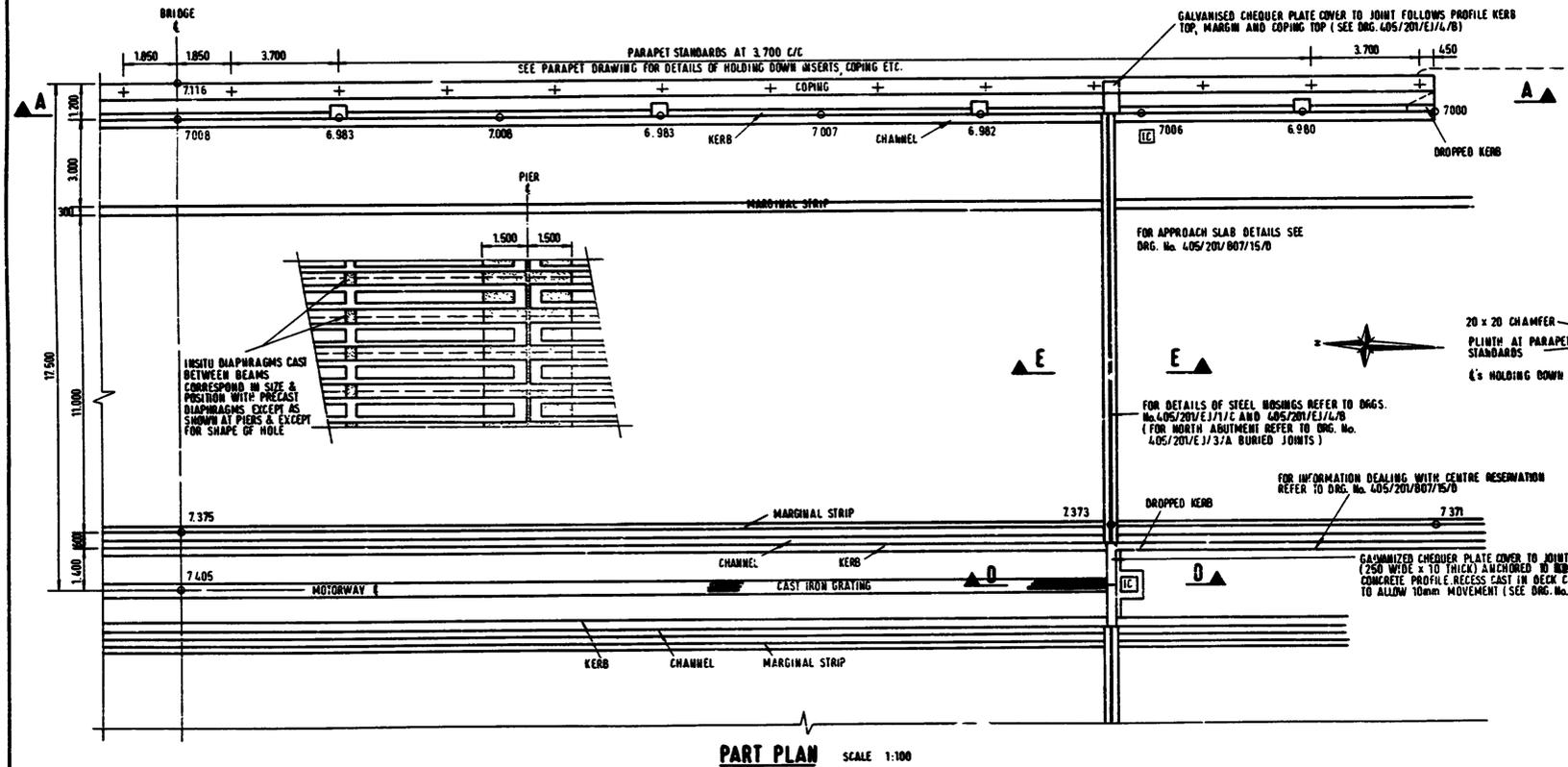
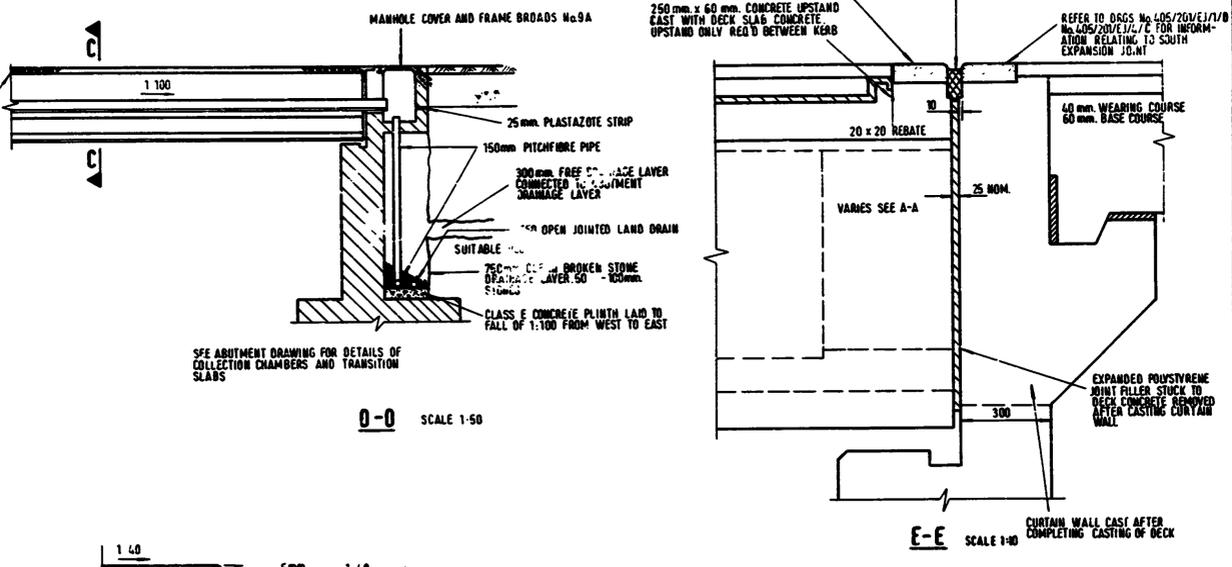
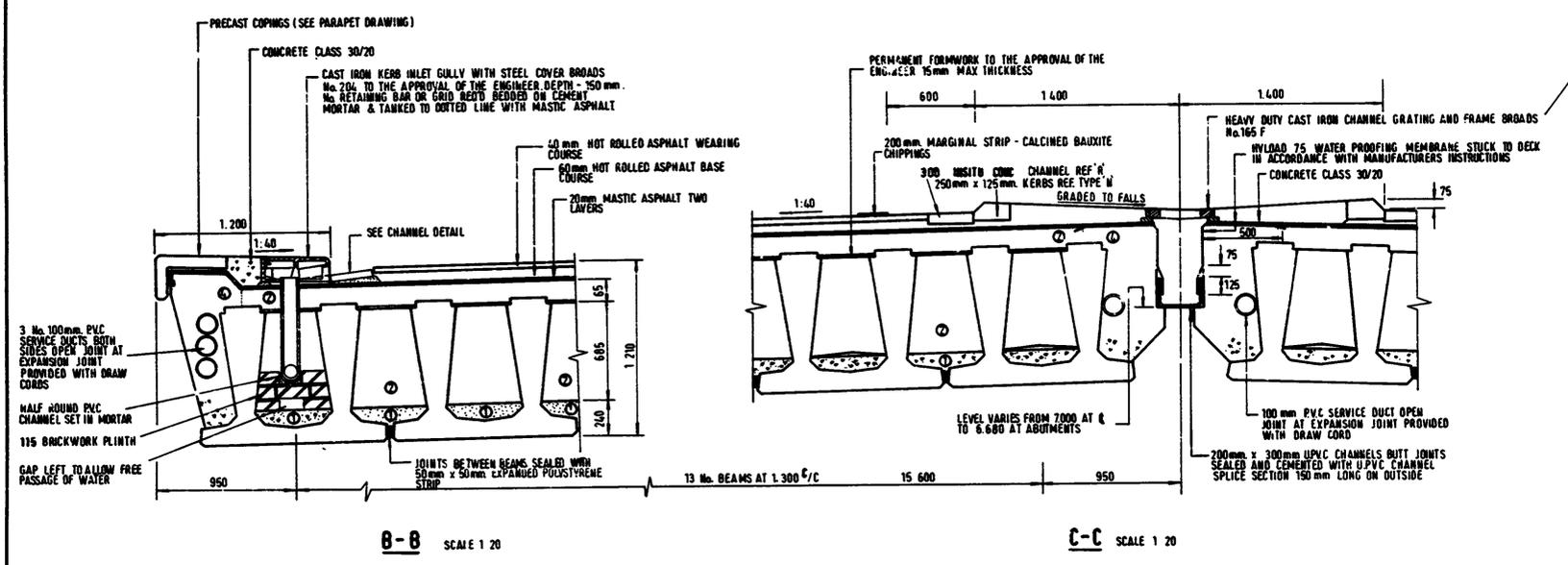
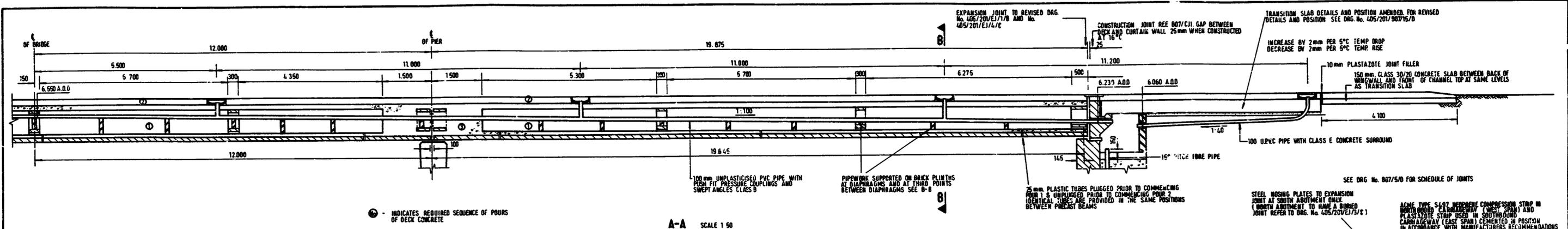
DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng., C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

LET	REVISION	DATE	BY
C	RE-DRAWN - AS-BUILT DRAWING	JUN 74	JWH
B	NOTE RE DRAINAGE TUBES EXTENDED		
A	R2001 & R2002 - EACH WEB NOTE ADDED		
	AMENDMENT DETAILS		

BIRMINGHAM-EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
RIVER HUNTSPELL BRIDGE PRETENSIONED BEAMS

DRAWN PG.	DATE	SCALES			
		1:10	1:20	1:25	1:200
AS-BUILT DRAWING	JUNE 69				
TRACED	DATE				
J.W.	SEPT 75				
CHECKED	DATE				
AJP	JULY 70				

DRAWING No. 405/201/B07/9/C

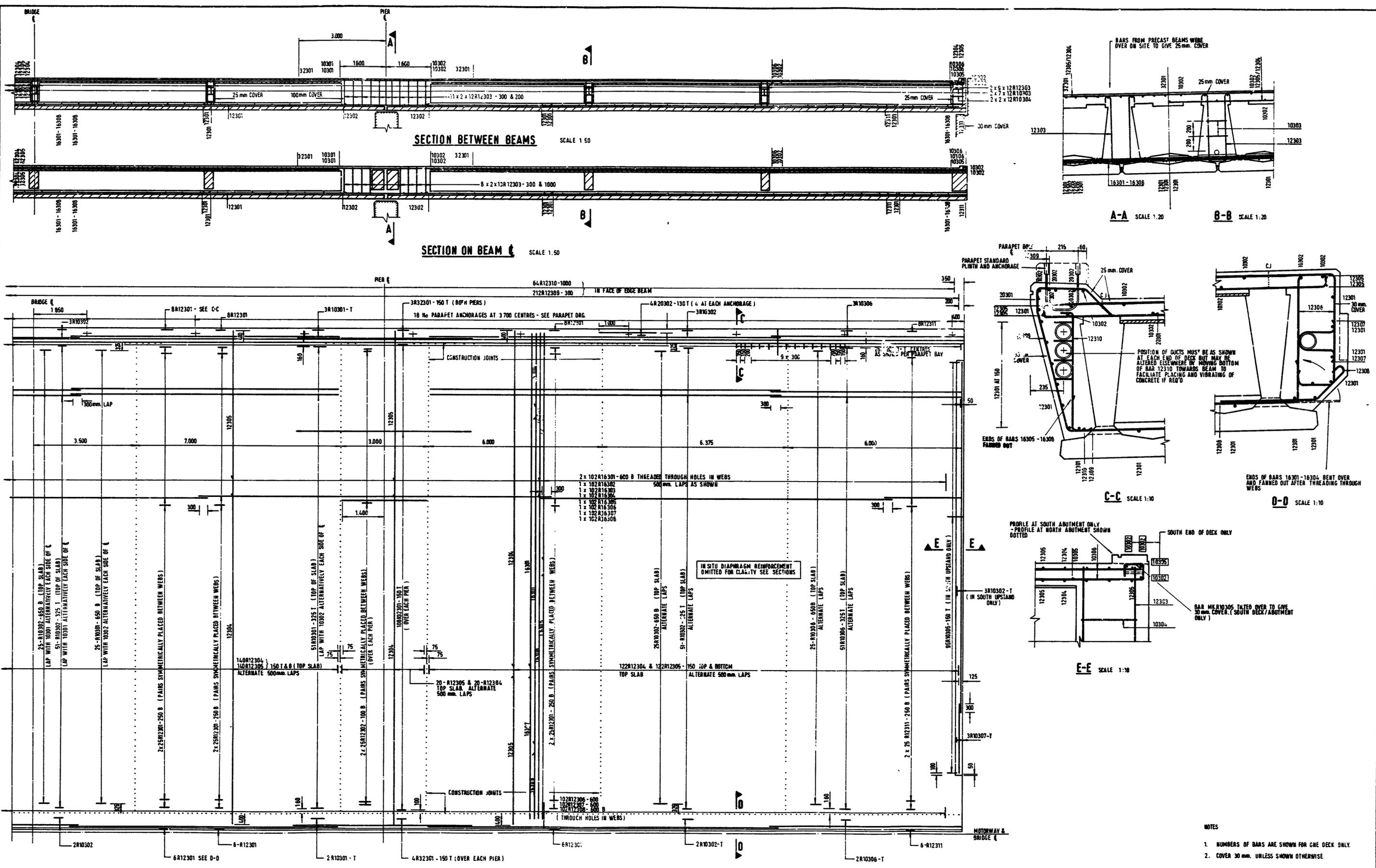


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 Director and Engineer P. G. Lyth B.Eng. C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

B	RE-DRAWN - "AS BUILT" DRAWING	J.N.H.	SEPT. 73
C	REVISED EXPANSION JOINT INFORMATION ADDED AND POSITION OF KERBS IN CENTRAL RESERVATION AMENDED	E.J.R.	FEB. 73
B	TRANSITION SLAB DETAILS AND POSITION AMENDED	P.W.	JULY 72
A	END PARAPET BAY ALTERED AND STAYPUT SIZE AMENDED	P.W.	JAN. 72
LET	AMENDMENT DETAILS	BY	DATE

BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
HUNTSPELL RIVER BRIDGE DECK LAYOUT

"AS-BUILT" DRAWING	DRAWN PG	DATE JUNE 69	SCALE 1:50
	TRACED I.W.	DATE SEPT. 75	SCALE 1:100
	CHECKED	DATE JULY 70	SCALE 1:20
	A.J.P.	DATE	SCALE 1:100
DRAWING No. 405/201/807/10/D			



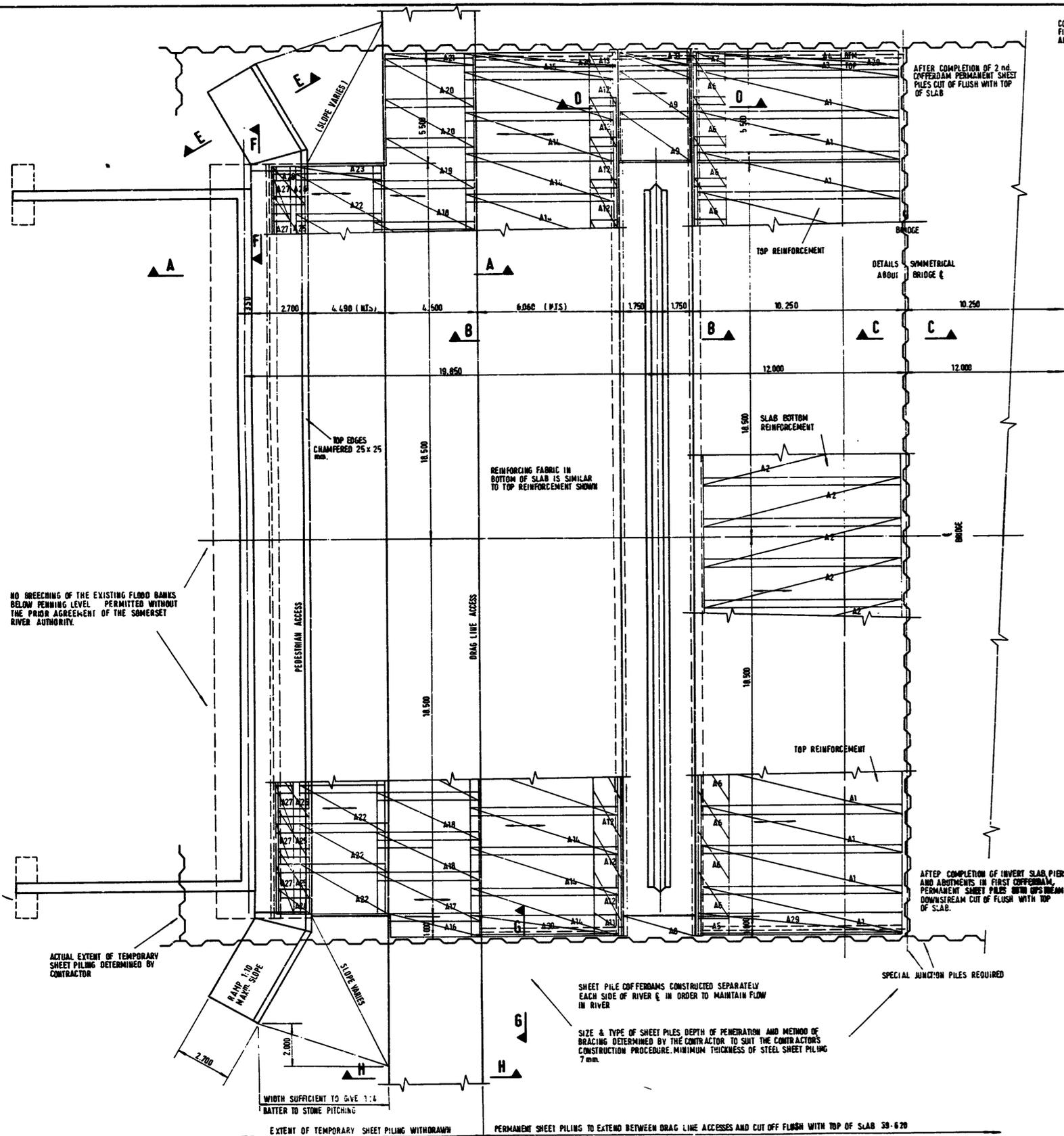
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 SOMERSET COUNTY COUNCIL SUB-UNIT

D	REDRAWN "AS-BUILT" DRAWING	J.W.H.	SEPT 73
C	SECTION E-E CORRECTED		
B	EXPANSION JOINT REVISION INFORMATION ADDED	E.J.R.	FEB 73
A	STAYPUT SIZE AMENDED ON SECTION C-C	P.W.	JAN 72
LET	AMENDMENT DETAILS	BY	DATE

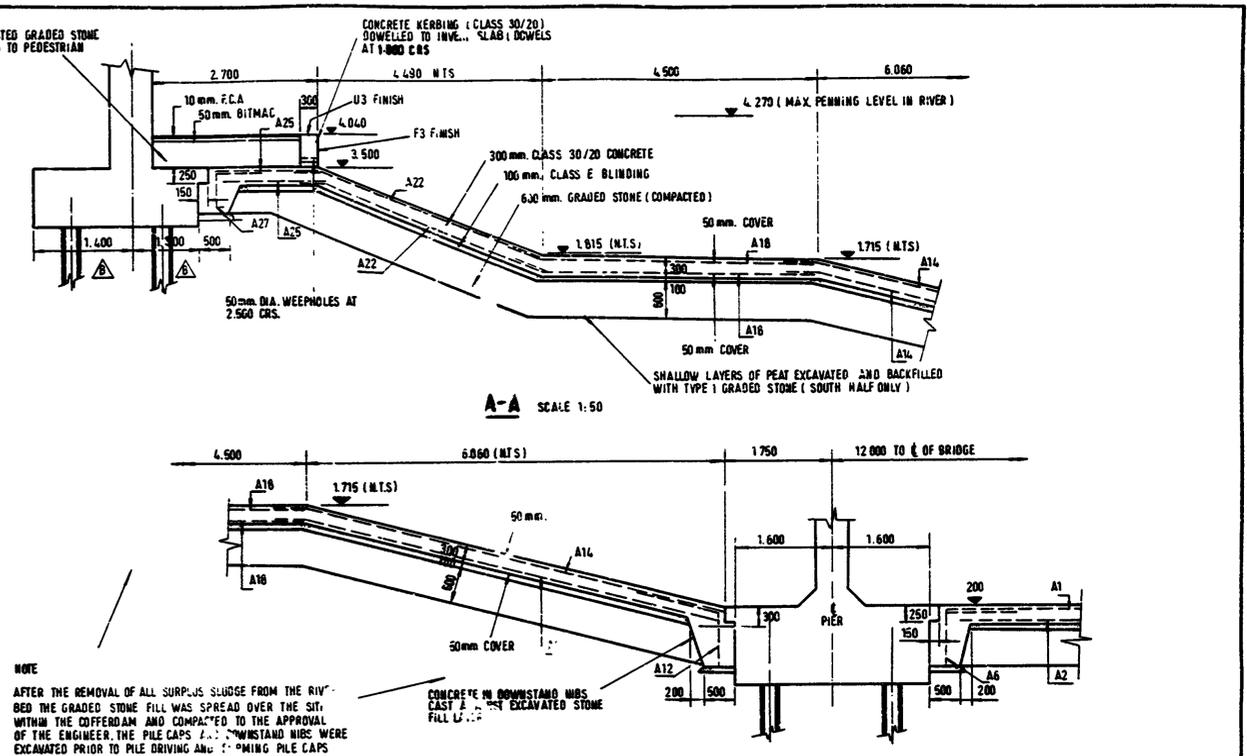
BIRMINGHAM-EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
HUNTSPILL RIVER BRIDGE DECK REINFORCEMENT

"AS-BUILT" DRAWING	DRAWN P.G.	DATE	SCALE
	TRACED J.W.	DATE SEPT 75	1:50 1:20 1:10
	CHECKED A.J.P.	DATE AUG 70	DRAWING No
			405/201/807/11/0

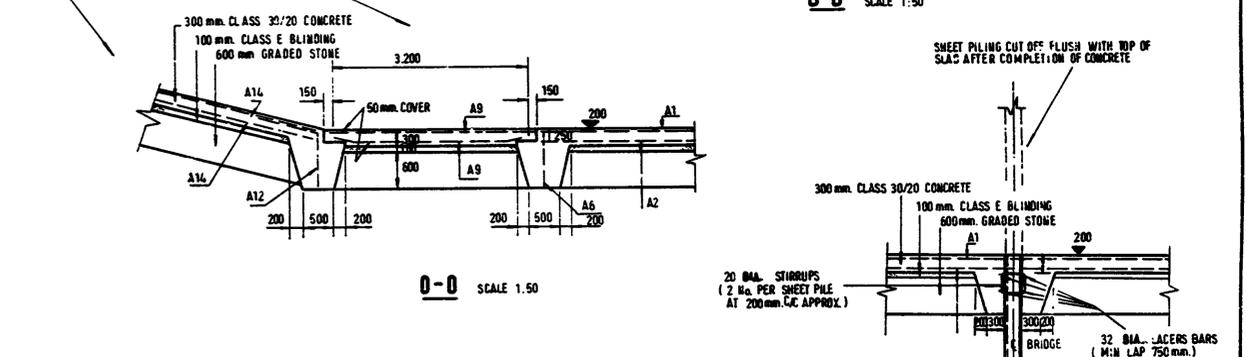
METRIC UNITS



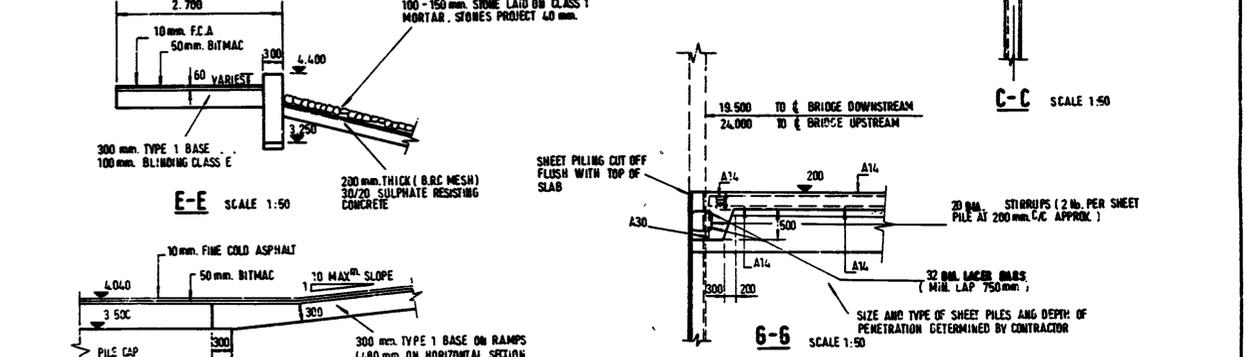
PART PLAN SHOWING INVERT SLAB SCALE 1:100



A-A SCALE 1:50



B-B SCALE 1:50



C-C SCALE 1:50



E-E SCALE 1:50



F-F SCALE 1:50

H-H SCALE 1:50 (DRAGLINE ACCESS OUTSIDE COFFERDAM)

- NOTES**
1. INVERT SLAB GENERALLY REINFORCED WITH TWO LAYERS (1/3 TOP AND 2/3 BOT.) OF 25 REE B503 STRUCTURAL FABRIC
 2. CONCRETE FOR INVERT SLAB CLASS 30/20
 3. CONCRETE FOR RAMPED FOOTPATHS AND DRAGLINE ACCESSES OUTSIDE OF COFFERDAMS CLASS 20/40
 4. MINIMUM COVER TO REINFORCEMENT 50mm.
 5. FABRIC REINFORCEMENT HAVE 400mm MINIMUM SIDE AND END LAPS
 6. ——— DENOTES DIRECTION OF MAIN WIRES IN FABRIC REINFORCEMENT SHEETS
 7. FABRIC REINFORCEMENT IS DETAILED ON SCHEDULES Nos. 1-4 INC.

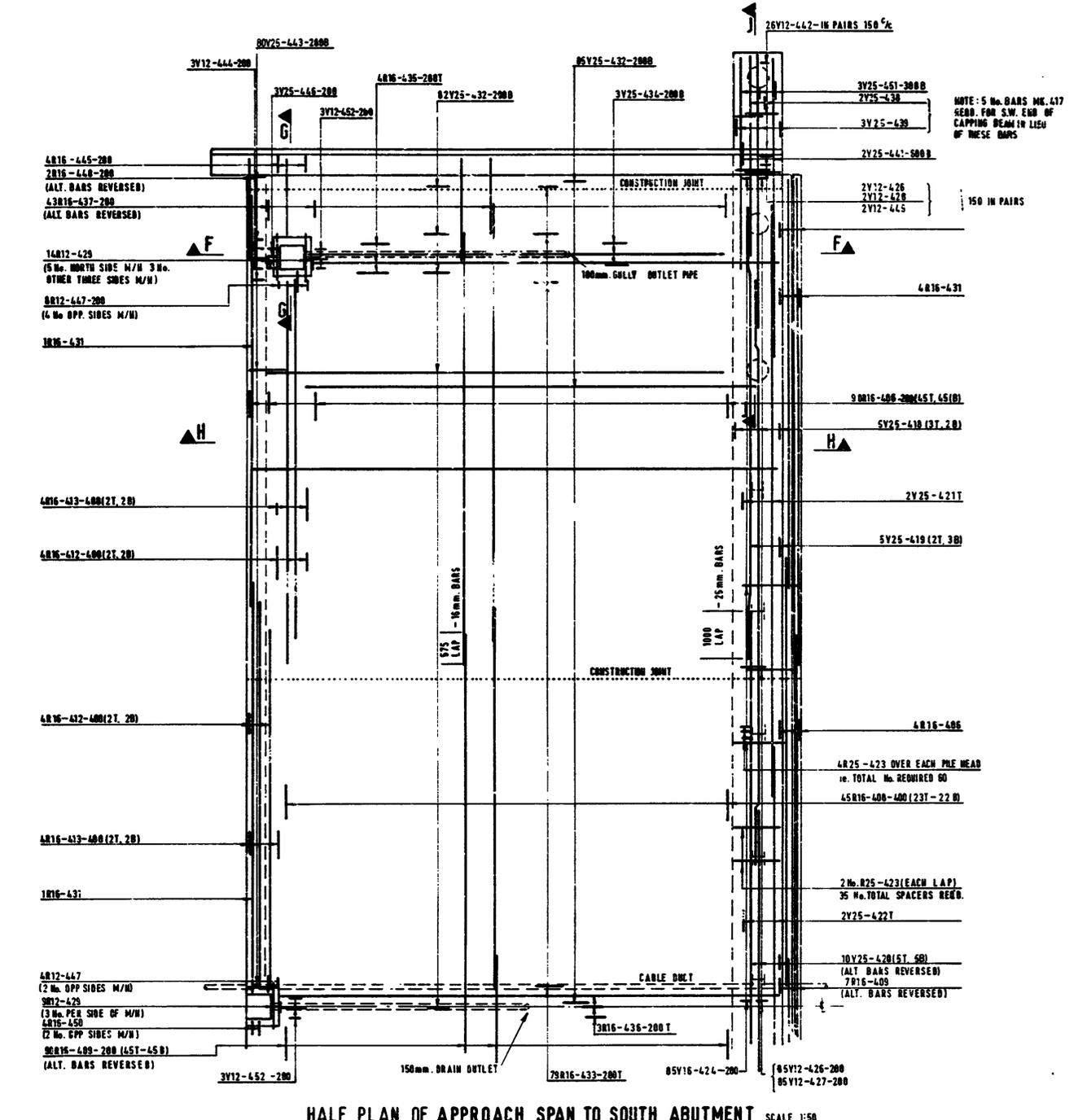
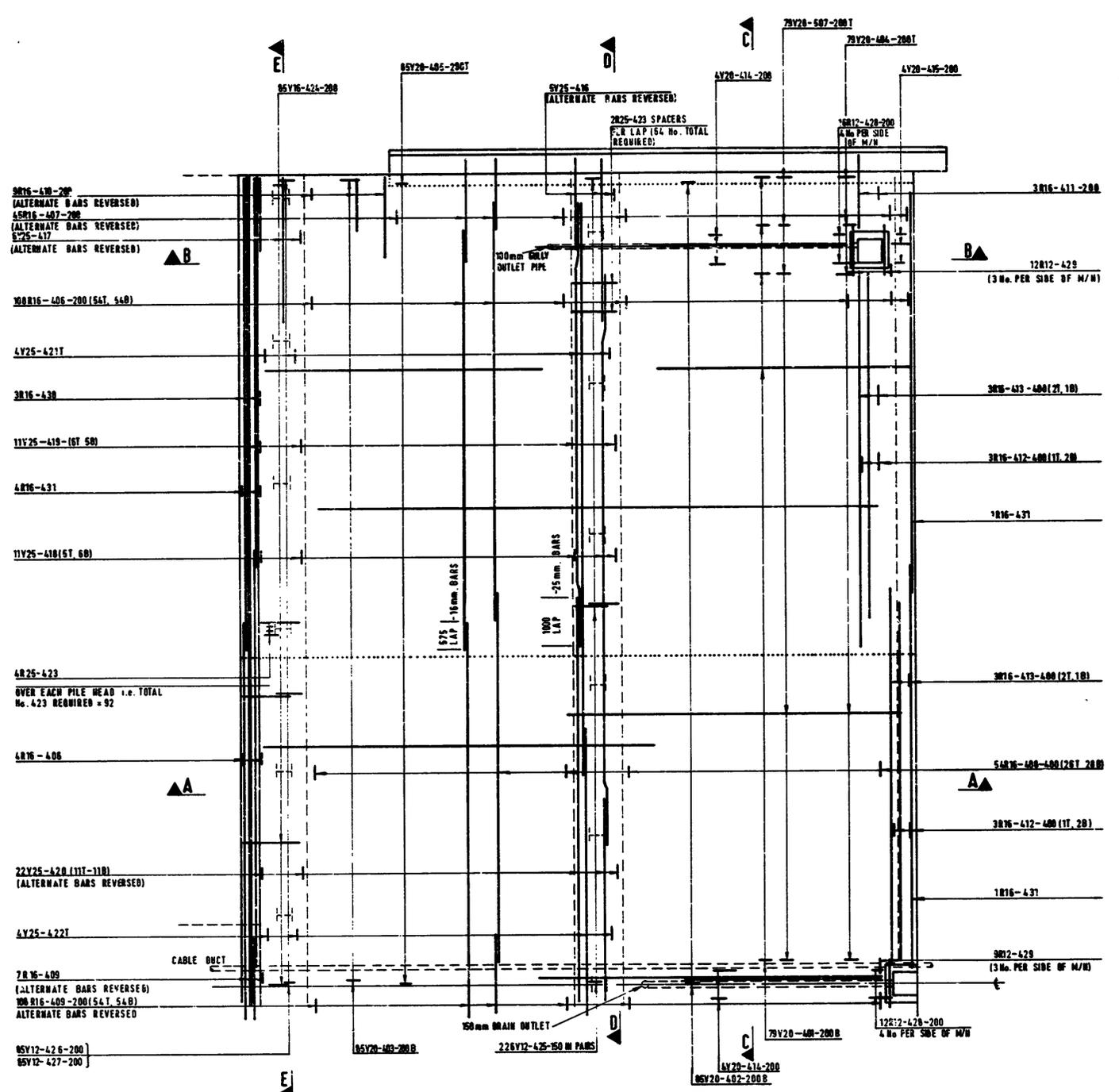
METRIC UNITS

DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P.G. Lyth B.Eng. C.Eng.
 SOMERSET COUNTY COUNCIL SUB-UNIT

C	RE-DRAWN AS BUILT DRAWING	J.M.K.	MARCH 73
B	SECTION A-A AMENDED	P.D.M.	JULY 72
A	DRAG LINE ACCESS LEVELS AMENDED	P.W.	MAY 72
LET	AMENDMENT DETAILS	BY	DATE

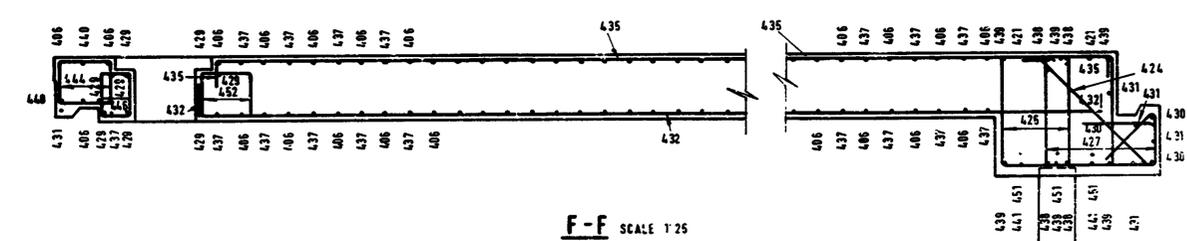
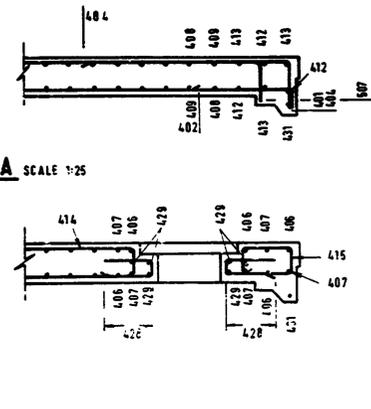
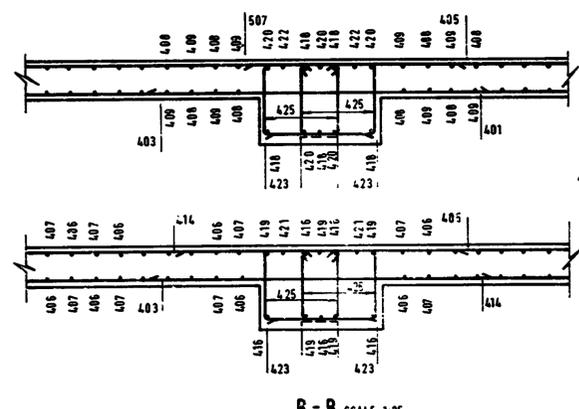
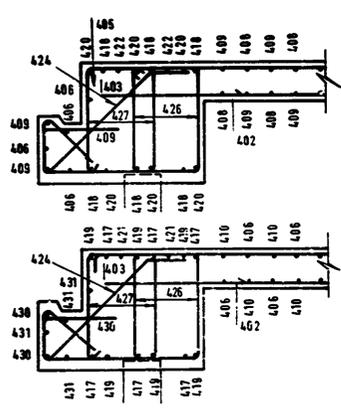
BIRMINGHAM - EXETER MOTORWAY M5
SECTION: EDITHMEAD TO DUNBALL
HUNTSPELL RIVER BRIDGE DETAILS OF INVERT SLAB TO RIVER BED AND BANKS

AS-BUILT DRAWING	DRAWN	KB	DATE	SCALES 1:100 1:50
	TRACED	J.W.	DATE OCT. 75	
	CHECKED	A.J.P.	DATE JULY 76	
DRAWING No 405/201/807/14/C				



HALF PLAN OF APPROACH SPANS TO NORTH ABUTMENT SCALE 1:50

HALF PLAN OF APPROACH SPAN TO SOUTH ABUTMENT SCALE 1:50



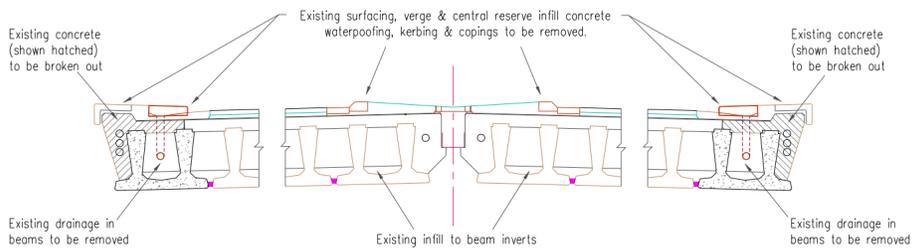
- NOTES
- 1 CONCRETE COVER :- BOTTOM STEEL 45mm, TOP STEEL 35mm.
 - 2 THIS DRAWING TO BE READ IN CONJUNCTION WITH DRG. No. 405/201/807/17/A
 - 3 FOR CONSTRUCTIONAL DETAILS OF APPROACH SPANS SEE DRG. No. 405/201/807/16/B
 - 4 SLAB SYMMETRICAL ABOUT LONGITUDINAL C/O OF BARS SHOWN GENERALLY APPLIES TO THE HALF PLAN SHOWN ONLY.
 - 5 REINFORCEMENT ADJUSTED LOCALLY WHERE NECESSARY TO ACCOMMODATE DRAINAGE OUTLET PIPES.

METRIC UNITS

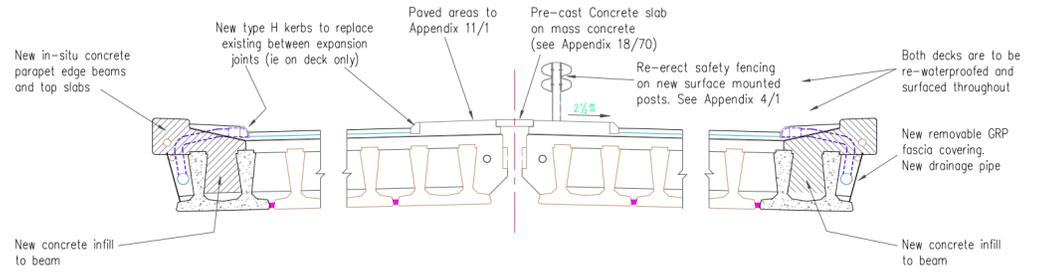
DEPARTMENT OF THE ENVIRONMENT. S.W.R.C.U.
 Director and Engineer P. G. Lyth B.Eng. C.Eng
 SOMERSET COUNTY COUNCIL SUB-UNIT

BIRMINGHAM - EXETER MOTORWAY M5
 SECTION: EDITHMEAD TO DUNBALL
 HUNTSPELL RIVER BRIDGE
 APPROACH SPAN REINFORCEMENT 1

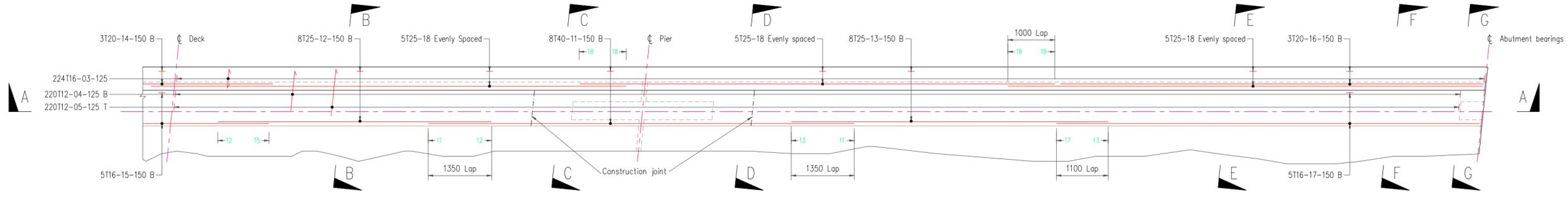
DRAWN	DATE	SCALES	
		1:50	1:25
AS-BUILT DRAWING	APRIL 72		
TRACED M.S.	NOV 75		
CHECKED J.P.T.	JUNE 72		
		DRAWING No. 405/201/807/16/A	



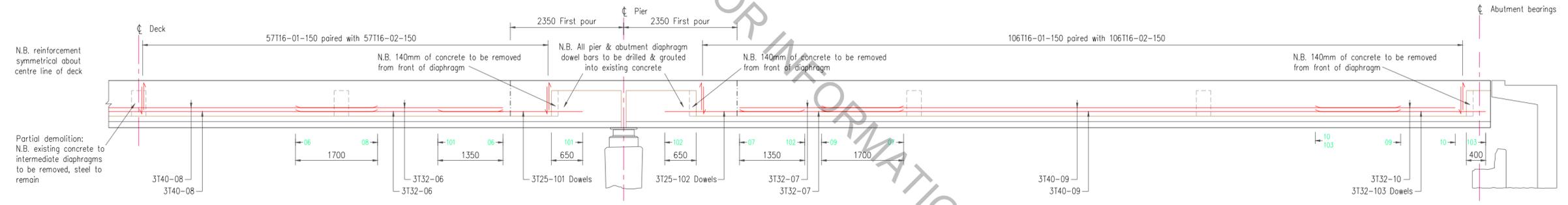
SECTION THROUGH EXISTING DECK - PARTIAL DEMOLITION (1:50)



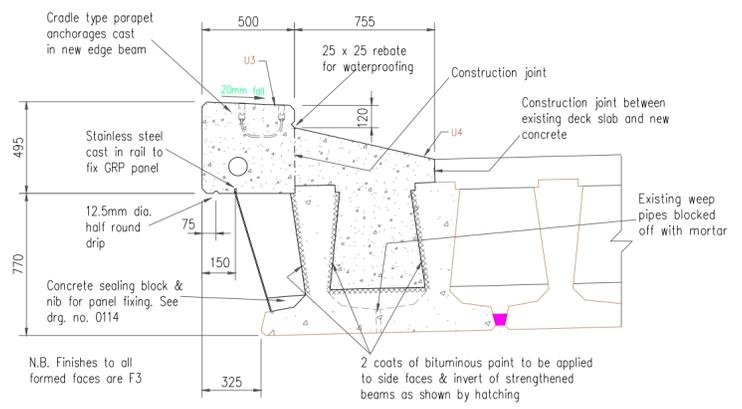
SECTION THROUGH DECK (1:50)



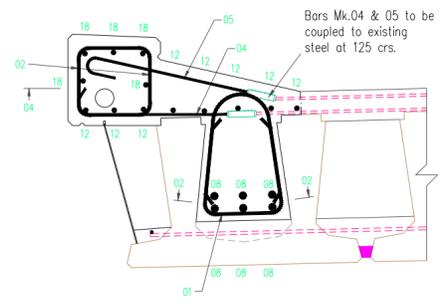
PLAN VIEW ON WEST EDGE BEAM - REINFORCEMENT DETAILS (1:50)
N.B. EAST EDGE BEAM SIMILAR



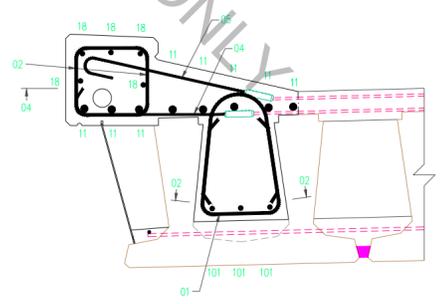
SECTION A-A THROUGH EDGE BEAM - TYPICAL (1:50)



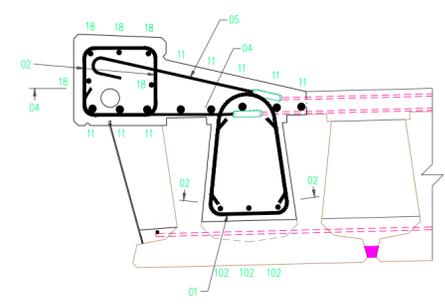
OUTLINE DETAIL OF WEST EDGE BEAM
N.B. EAST EDGE BEAM SIMILAR



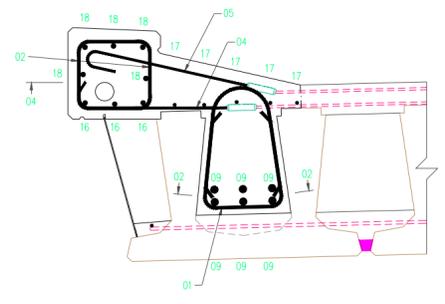
SECTION A-A (1:20)



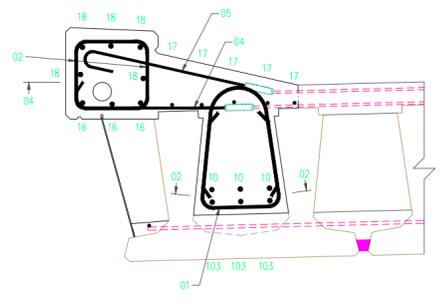
SECTION B-B (1:20)



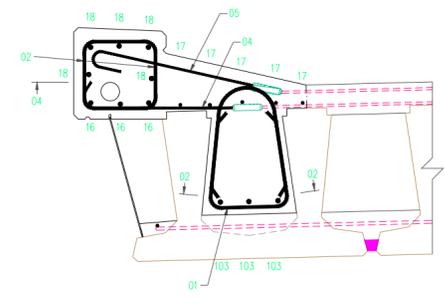
SECTION C-C (1:20)



SECTION D-D (1:20)



SECTION E-E (1:20)

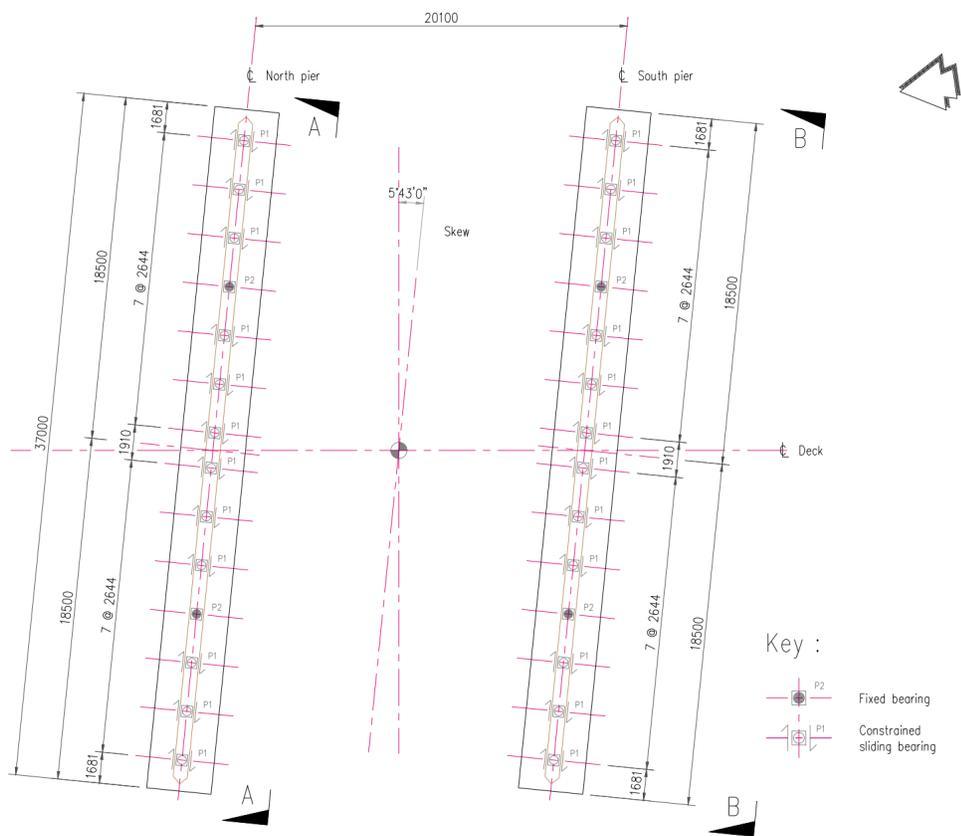


SECTION F-F (1:20)

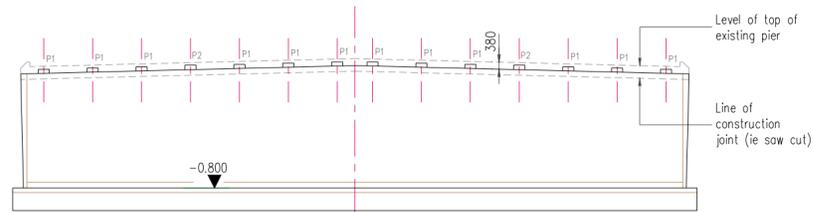
NOTES

- This drawing is to be read in conjunction with the following drawings:- BP1035.015-0101 to 0121
- Concrete cover to be :- 30mm min. generally
50mm parapet edge beam
- Concrete to be grade 50/20 - In new parapet edge beam
40/20 - Elsewhere
- Reinforcement (deformed type 2) to be bs.4449 or 4461 with characteristic strength not less than 460N/mm² for bars marked "T"
- Reinforcement may require local adjustment to accommodate existing reinforcement, cast in fittings and parapet anchorages
- Couplers to have a British Board of Agreement Certificate and are to be approved by the Engineer
- Abbreviations :- T - Top B - Bottom
- Pour sequence:- Concrete in areas over piers as shown on the drawing to be placed first.

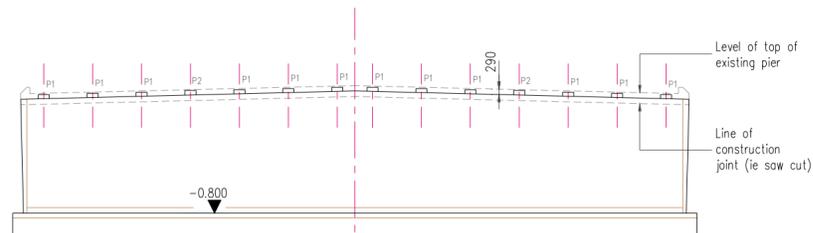
<p>South West Consultancy The Crescent, Taunton, Somerset TA1 4XE TEL (01823) 353433 FAX (01823) 353430</p>	
Client	
Title	<p>M5 RIVER BRUE BRIDGE REFURBISHMENT AND BEARING REPLACEMENT DECK STRENGTHENING AND PARTIAL DEMOLITION DETAILS</p>
designed	PFS
date	JAN 97
drawn	MJE
date	JAN 97
checked	
date	
approved	
date	
Revisions	<p>A : June 97:MJE: First Issue B : March 99: SW : As Built</p>
Scale	AS SHOWN
ACAD Ref	br-deck.dwg
DISC Ref	\\drawings\BP1035_015
Survey Disc Ref	
Drng no	BP1035.015-0102A



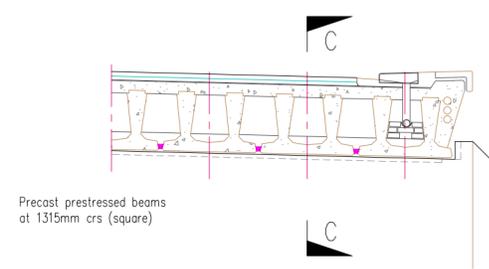
PLAN ON PIERS (1:200)



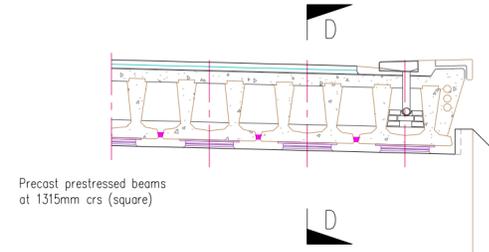
ELEVATION A-A ON NORTH PIER (1:200)



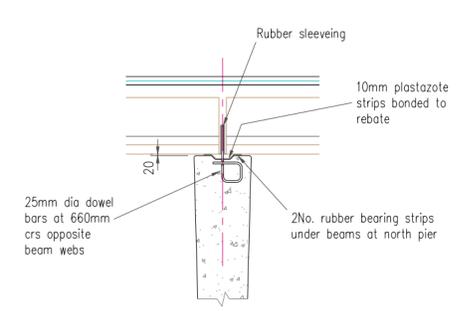
ELEVATION B-B ON SOUTH PIER (1:200)



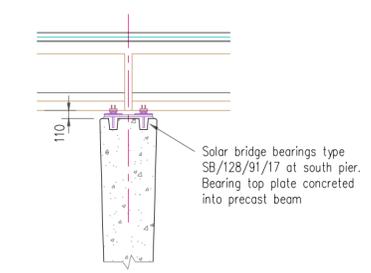
SECTION THROUGH EXISTING DECK AT EAST END OF NORTH PIER (1:50)



SECTION THROUGH EXISTING DECK AT EAST END OF SOUTH PIER (1:50)

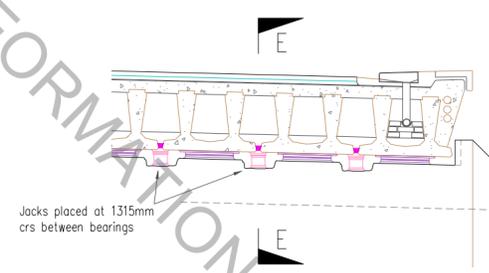


SECTION C-C (1:50)

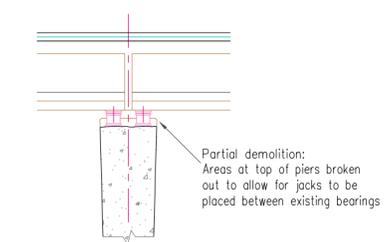


SECTION D-D (1:50)

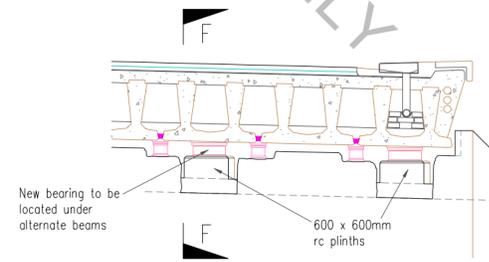
DETAILS OF EXISTING BEARINGS



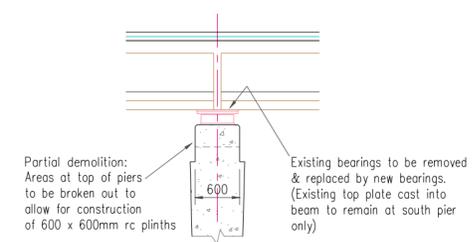
STAGE 1 (1:50)



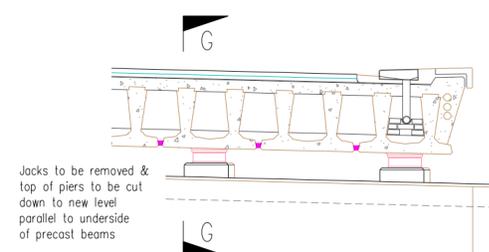
SECTION E-E (1:50)



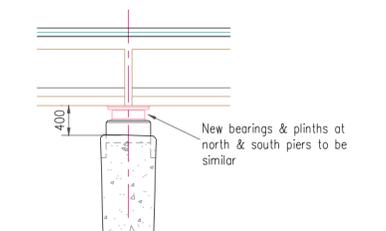
STAGE 2 (1:50)



SECTION F-F (1:50)



STAGE 3 (1:50)



SECTION G-G (1:50)

EXAMPLE OF METHOD OF REPLACING BEARINGS

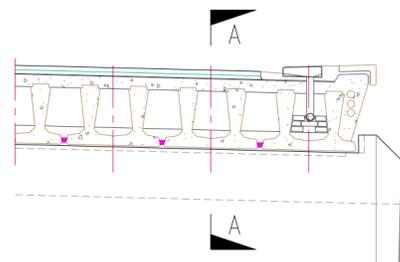
- NOTES**
- This drawing is to be read in conjunction with the following drawings BP1035.015-258-0101 to 0121
 - All bearings to be set horizontally.
 - Method of replacing bearings is for example only - temporary jacking system to be designed by contractor & is subject to engineers approval.
 - The jacks shall be placed at centres along the piers no greater than those of the new bearings.

WS Atkins
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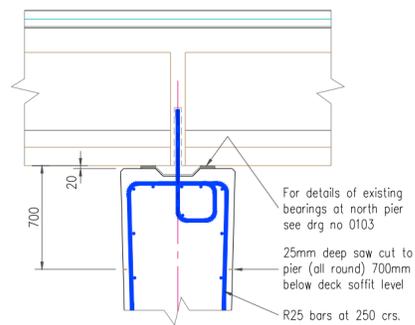
Client	
Title	M5 RIVER BRUE BRIDGE REFURBISHMENT AND BEARING REPLACEMENT PIER PARTIAL DEMOLITION
designed	PFS
date	JAN 97
drawn	MJE
date	JAN 97
checked	
date	
approved	
date	

- Revisions**
- A : June 97: MJE: First Issue
 - B : March 99: SW : As Built

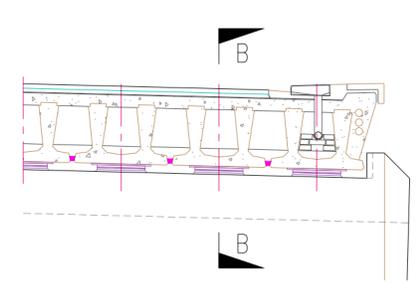
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ACAD Ref	br-pier1.dwg
DISC Ref	t:\drawings\BP1035_015
Survey Disc Ref	
Drng no	



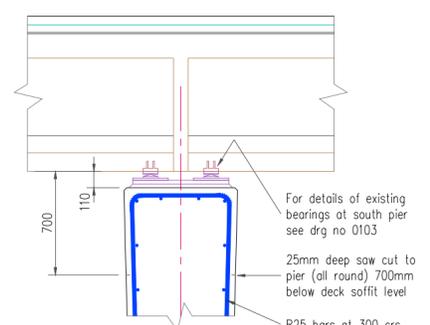
SECTION THROUGH EXISTING DECK AT EAST END OF NORTH PIER (1:50)



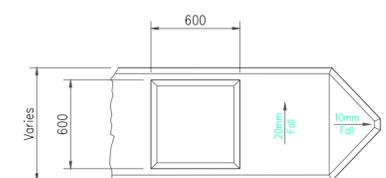
SECTION A-A (1:25)



SECTION THROUGH EXISTING DECK AT EAST END OF SOUTH PIER (1:50)

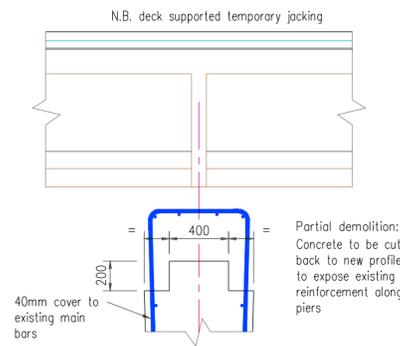


SECTION B-B (1:25)

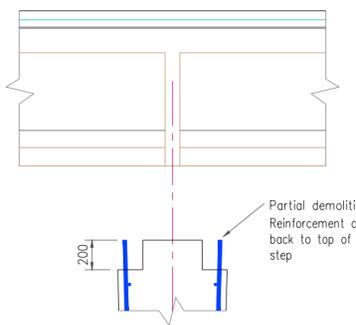


PLAN ON PIER END (1:25)

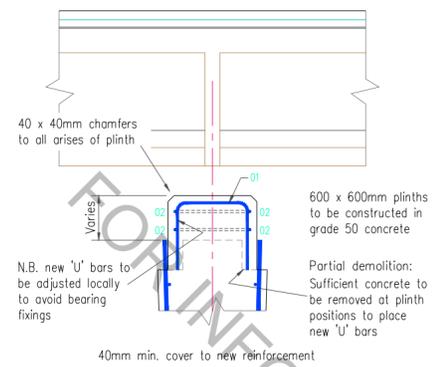
- NOTES**
- This drawing shall be read in conjunction with the following drawings: BP1035.015-0101 to 0121
 - All bearings and auxiliary top plates will be supplied by others.
 - The contractor shall arrange to collect the bearings and auxiliary top plates from the address given in Appendix 21/70
 - The auxiliary plates will be supplied without the fixing holes having been drilled. The contractor shall determine suitable positions for the auxiliary top plate fixings to avoid existing reinforcement & then drill & grout-in as appropriate the 4No. fixings per plate
 - The position of such fixings shall be such to achieve, as near as possible, the centres shown in sections D-D & E-E.
 - Having located & secured the fixings the contractor shall accurately drill each auxiliary top plate to suit the fixing centres.
 - This drawing is to be revised when exact dimensions of bearings & auxiliary top plates are known
 - The Contractor shall note that the reinforcement/strands/links in the beams are congested & their position not precisely known. It will be necessary for the Contractor to use different methods of forming holes without damaging the steel in the beams. The position of fixing holes will vary from beam to beam. See Appendix 21/70
 - For painting of bearings after installation see Appendix 19/2



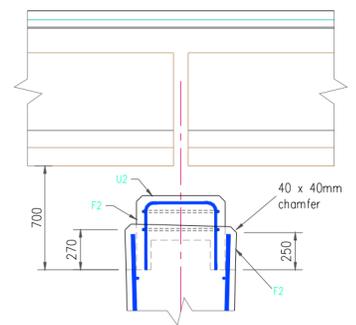
STAGE 1 (1:25)



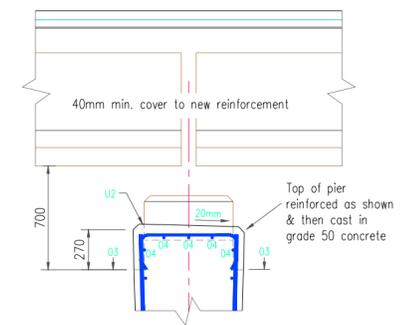
STAGE 2 (1:25)



STAGE 3 (1:25)



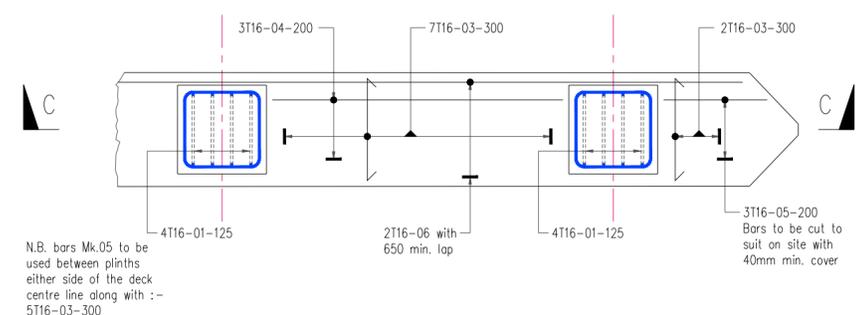
SECTION AT PLINTH



SECTION BETWEEN PLINTHS

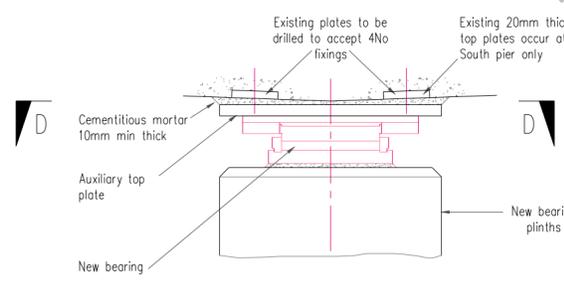
N.B. Top of piers to fall towards centre of bridge

STAGE 4 (1:25)

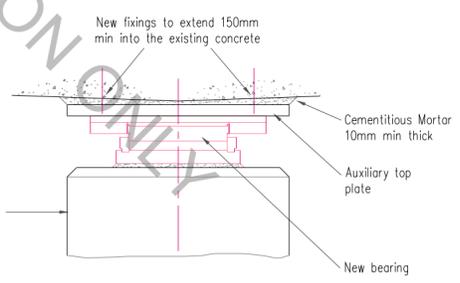


PLAN SHOWING REINFORCEMENT (1:25)

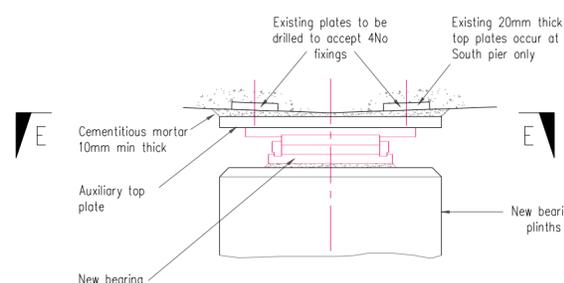
N.B. bars Mk.05 to be used between plinths either side of the deck centre line along with :- 5T16-03-300



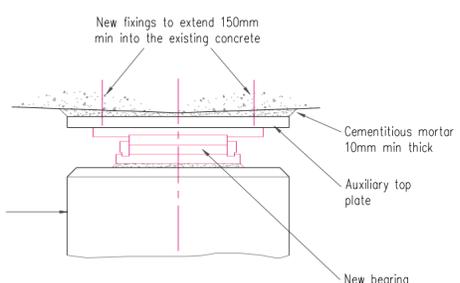
SLIDING BEARING (1:10)
(12No Thus)



SLIDING BEARING (1:10)
(12No Thus)



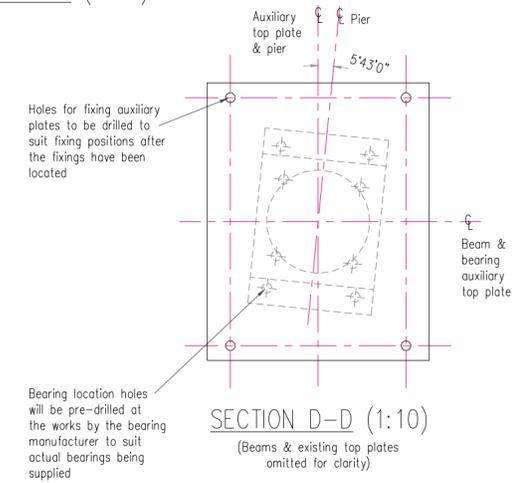
FIXED BEARING (1:10)
(2No Thus)



FIXED BEARING (1:10)
(2No Thus)

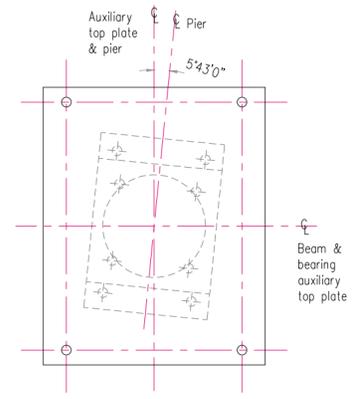
SECTIONS AT SOUTH PIER

SECTIONS AT NORTH PIER



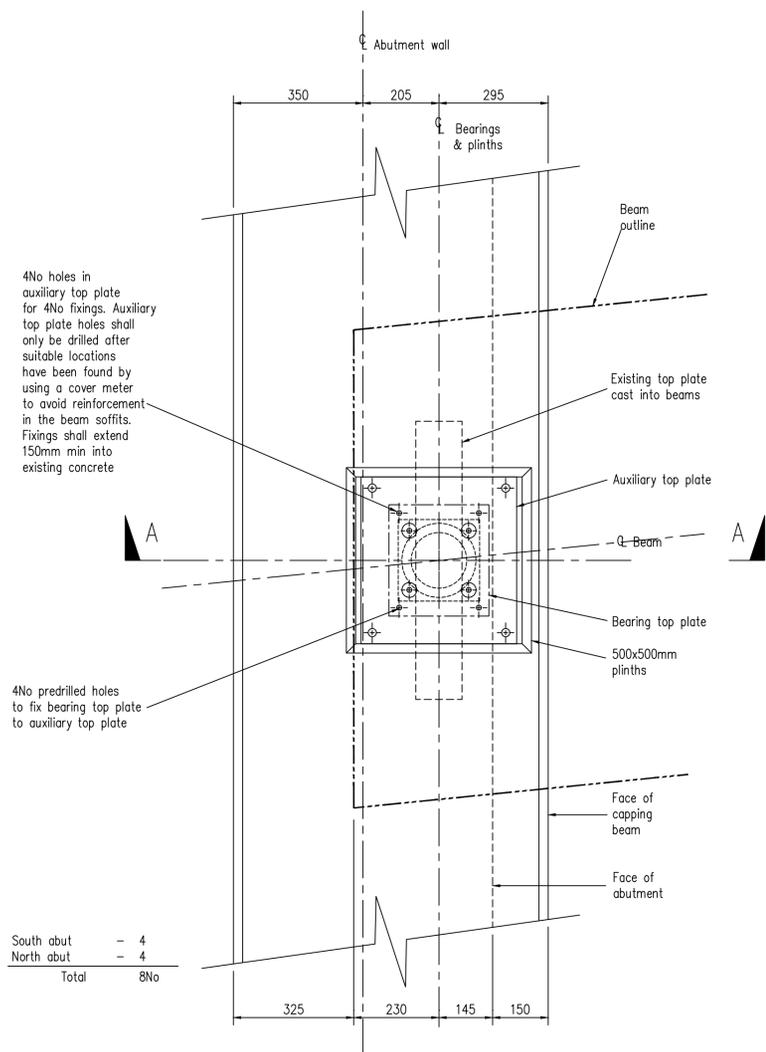
SECTION D-D (1:10)
(Beams & existing top plates omitted for clarity)

Bearing location holes will be pre-drilled at the works by the bearing manufacturer to suit actual bearings being supplied

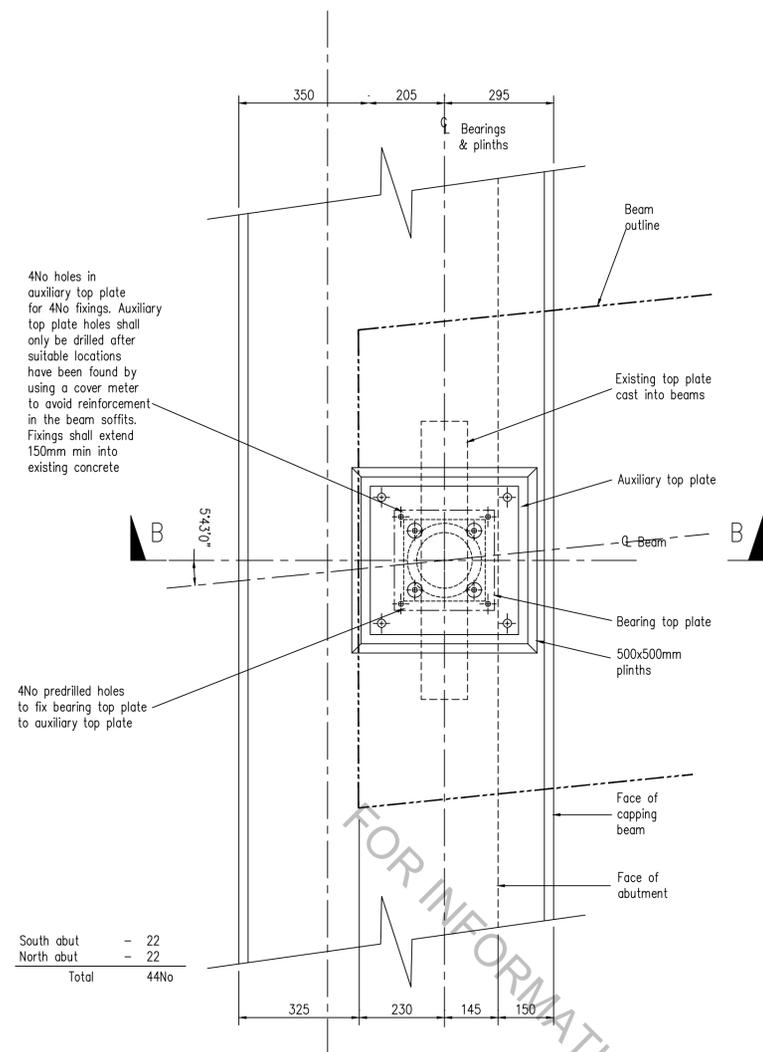


PLAN ON E-E (1:10)
(Beams & existing top plates omitted for clarity)

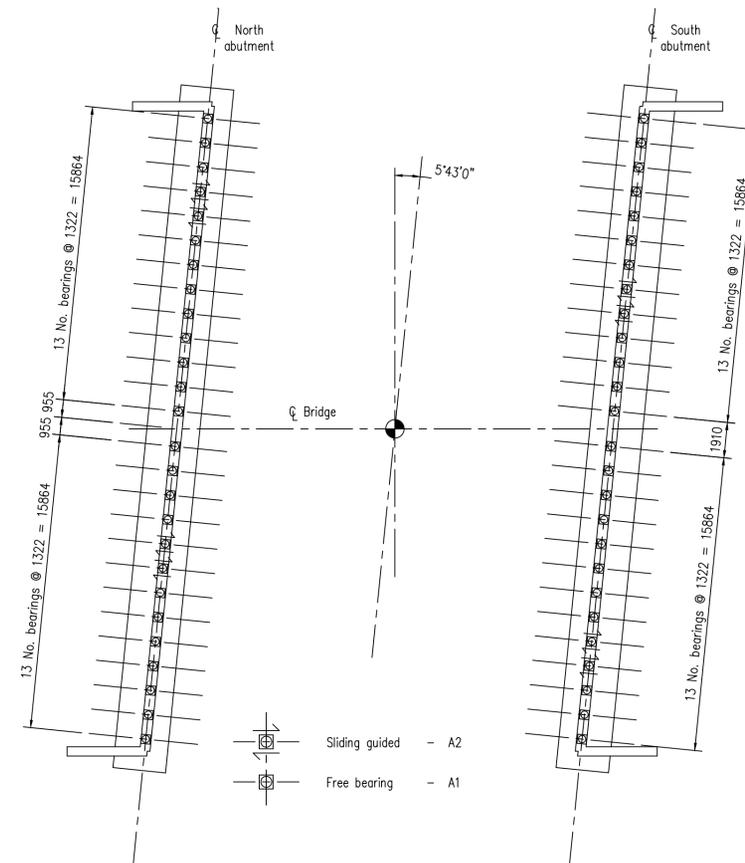
WS/Atkins South West Consultancy The Crescent, Taunton, Somerset TA1 4XE TEL (01823) 353433 FAX (01823) 353430	
Client	HIGHWAYS AGENCY
Title	M5 RIVER BRUE BRIDGE REFURBISHMENT AND BEARING REPLACEMENT PIER BEARING DETAILS
designed	date
drawn	date
checked	date
approved	date
Revisions	
A : June 97: MJE: First Issue	
B : March 99: SW : As Built	
Scale	AS SHOWN
ACAD Ref	br-pier2.dwg
DISC Ref	t:\drawings\BP1035_015
Survey Disc Ref	
Org no	BP1035.015-0104A



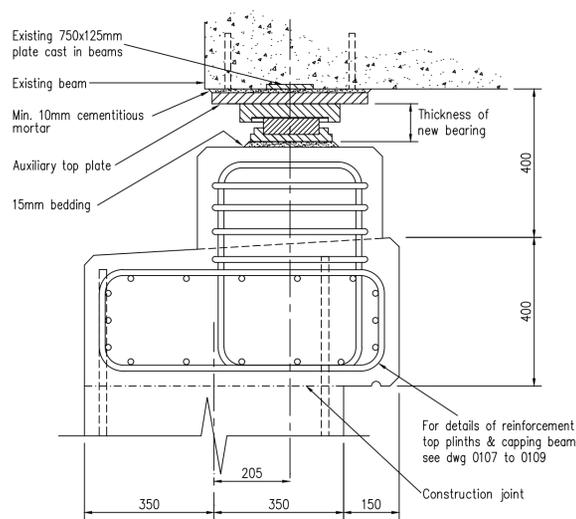
PLAN ON CONSTRAINED SLIDING BEARING MK 2 (1:10)



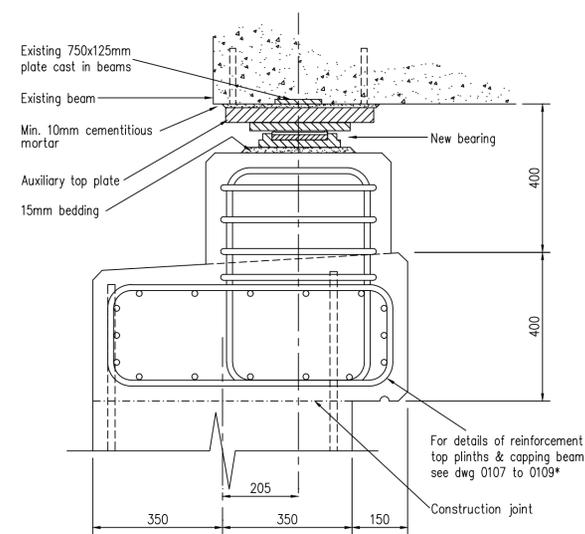
PLAN ON FREE SLIDING BEARING MK 1 (1:10)



PLAN SHOWING PROPOSED ARTICULATION (1:200)



SECTION A-A (1:10)



SECTION B-B (1:10)

NOTES

- Auxiliary top plates to be the same grade steel as bearing top plate & are to receive similar paint system as Appendix 19/2
- Fixing bolts to be of same material as supplied for bearings
- This drawing is to be read in conjunction with the following drawing: BP1035.015-0101 to 0121
- All bearings, auxiliary top plates & fixings to be supplied by others
- The auxiliary top plates are to be supplied without fixing holes. The contractor shall determine suitable positions for the auxiliary top plate fixings to avoid existing steel. The holes shall then be drilled accordingly. (See Appendix 21/70)
- The positions of fixing bolts shall be such to achieve, as near as possible, the centres shown in bearing plans
- Having located & secured the fixings the contractor shall accurately drill each auxiliary top plate to suit the fixing centres
- Exposed surfaces of existing top plates cast into beams to be painted after installation of new bearings in accordance with Appendix 19/2
- This drawing to be amended when exact dimensions of new bearings and auxiliary plates are known
- The Contractor shall note that the reinforcement/strands/links in the beams are congested & their position not precisely known. It will be necessary for the Contractor to use different methods of forming holes without damaging the steel in the beams. The position of fixing holes will vary from beam to beam. See Appendix 21/70

Client

Title
M5 RIVER BRUE BRIDGE

REFURBISHMENT AND BEARING REPLACEMENT

ABUTMENT BEARING DETAILS

designed *PFS* date *Feb 97*

drawn *MJE* date *Feb 97*

checked date

approved date

Revisions

A : June 97; MJE: First Issue

Scale
As Shown

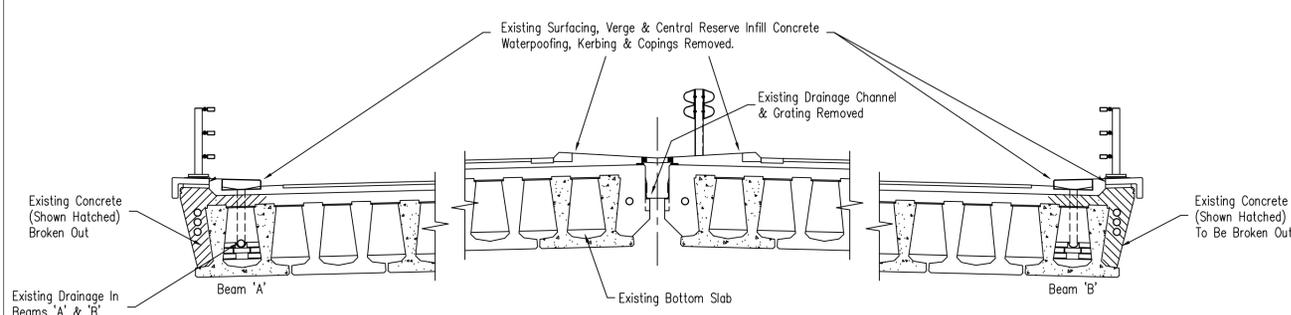
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br-abear.dwg

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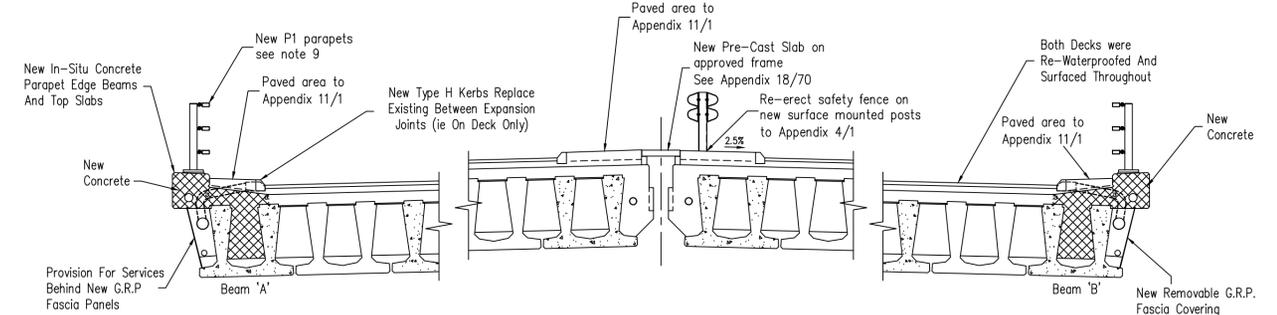
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Orig no

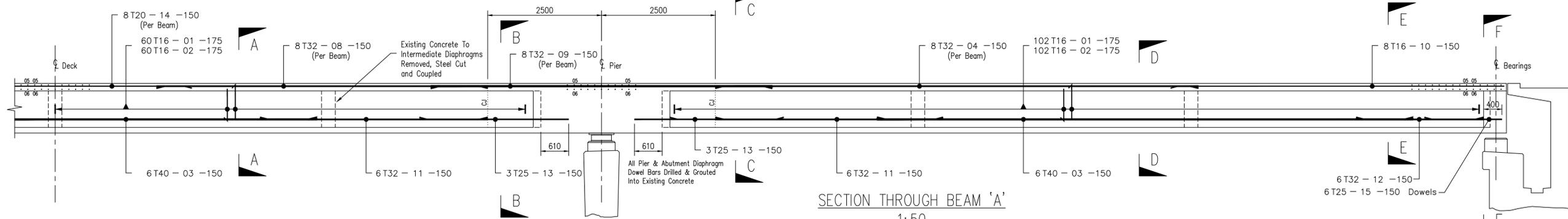
BP1035.015-0111A



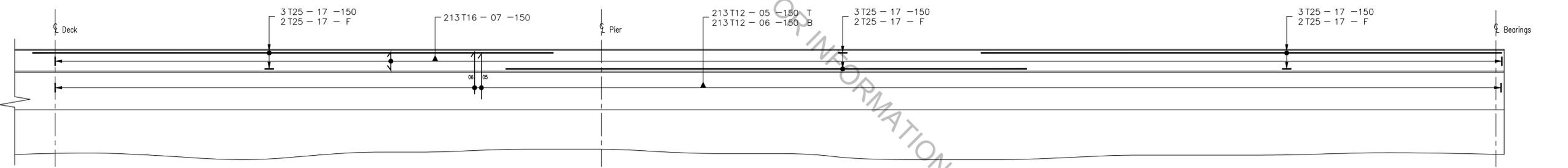
SECTION THROUGH EXISTING DECK SHOWING PARTIAL DEMOLITION
1:50



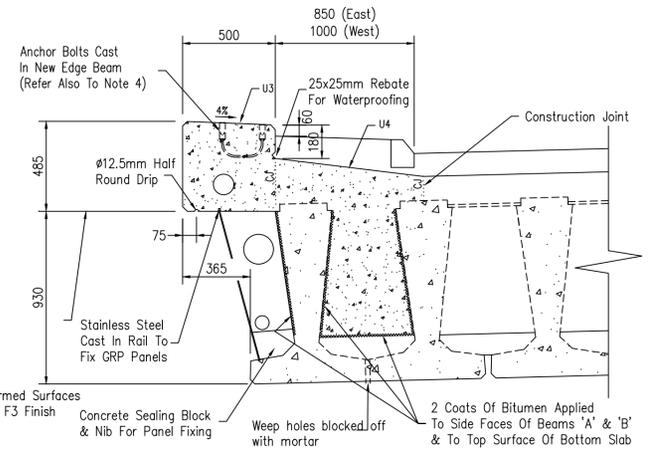
SECTION THROUGH NEW DECK
1:50



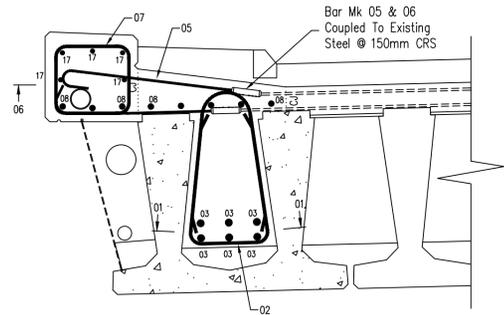
SECTION THROUGH BEAM 'A'
1:50
Beam 'B' Similar



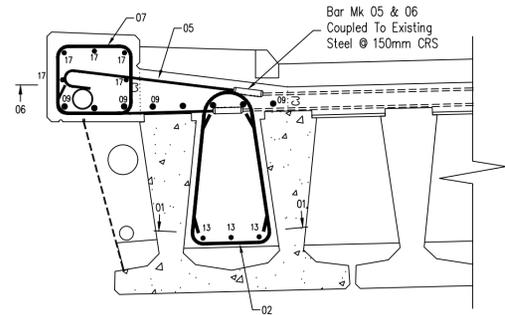
SECTION THROUGH BEAM 'A'
1:50
Beam 'B' Similar



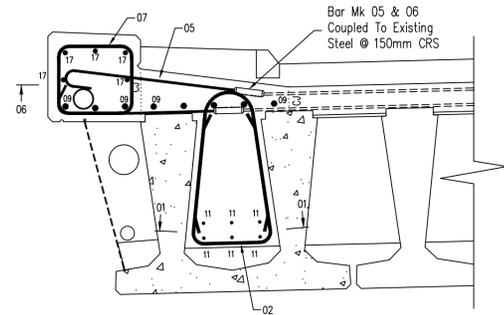
EDGE BEAM OUTLINE DETAIL
1:20



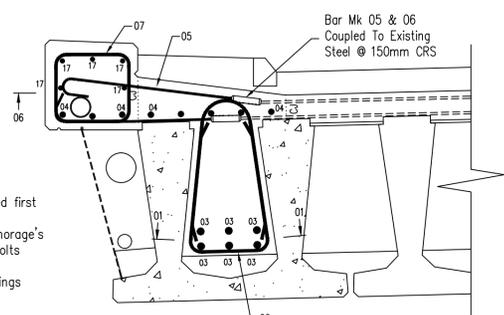
SECTION A-A
1:20



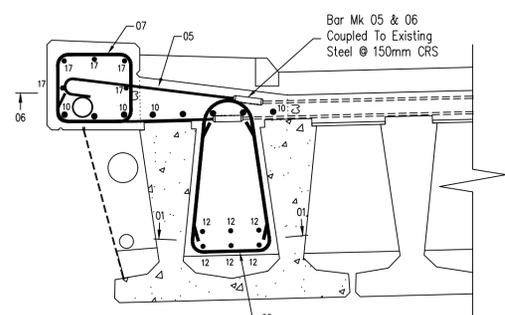
SECTION B-B
1:20



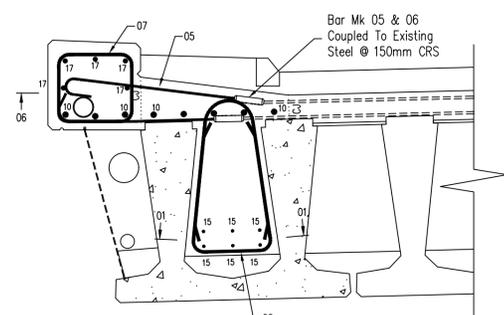
SECTION C-C
1:20



SECTION D-D
1:20



SECTION E-E
1:20



SECTION F-F
1:20

NOTES

- This drawing is to be read in conjunction with the following:
BP1035.016-0201 to 0221
- Concrete Cover To Be As Follows Unless Otherwise Stated:
30mm Generally (Minimum)
50mm Top Faces & Outside Face Of Parapet Upstand Beams
- Abbreviations:
T - Top
B - Bottom
F - Faces
- Reinforcement (Deformed Type 2) To BS4449 Or 4461 With Characteristic Strength Not Less Than 460N/mm² For Bars Marked 'I'
- Reinforcement May Require Local Adjustment To Accommodate Existing Reinforcement, Anchor Bolts etc
- Couplers To Be Approved By The Engineer For Compression & Tension Loads
- Concrete Grade To Be 50/20 In Parapet Upstand Beam 40/20 Elsewhere
- Pour sequence - Concrete in areas over piers to be placed first
- The Contractor shall provide new P1 steel parapets & anchorage's to BD 53/93. All posts shall be vertical & holding down bolts are to be stainless steel. The Contractor shall take all necessary site measurements and prepare fabrication drawings for the Engineers approval

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Client
HIGHWAYS AGENCY

Title
**M5 RIVER HUNSPILL BRIDGE
REFURBISHMENT AND BEARING REPLACEMENT
DECK STRENGTHENING DETAILS & PARTIAL DEMOLITION**

designed	PFS	date	Feb 97
drawn	AW	date	Feb 97
checked		date	
approved		date	

Revisions
A : June 97:AW: First Issue
C : March 99:SW: As Built

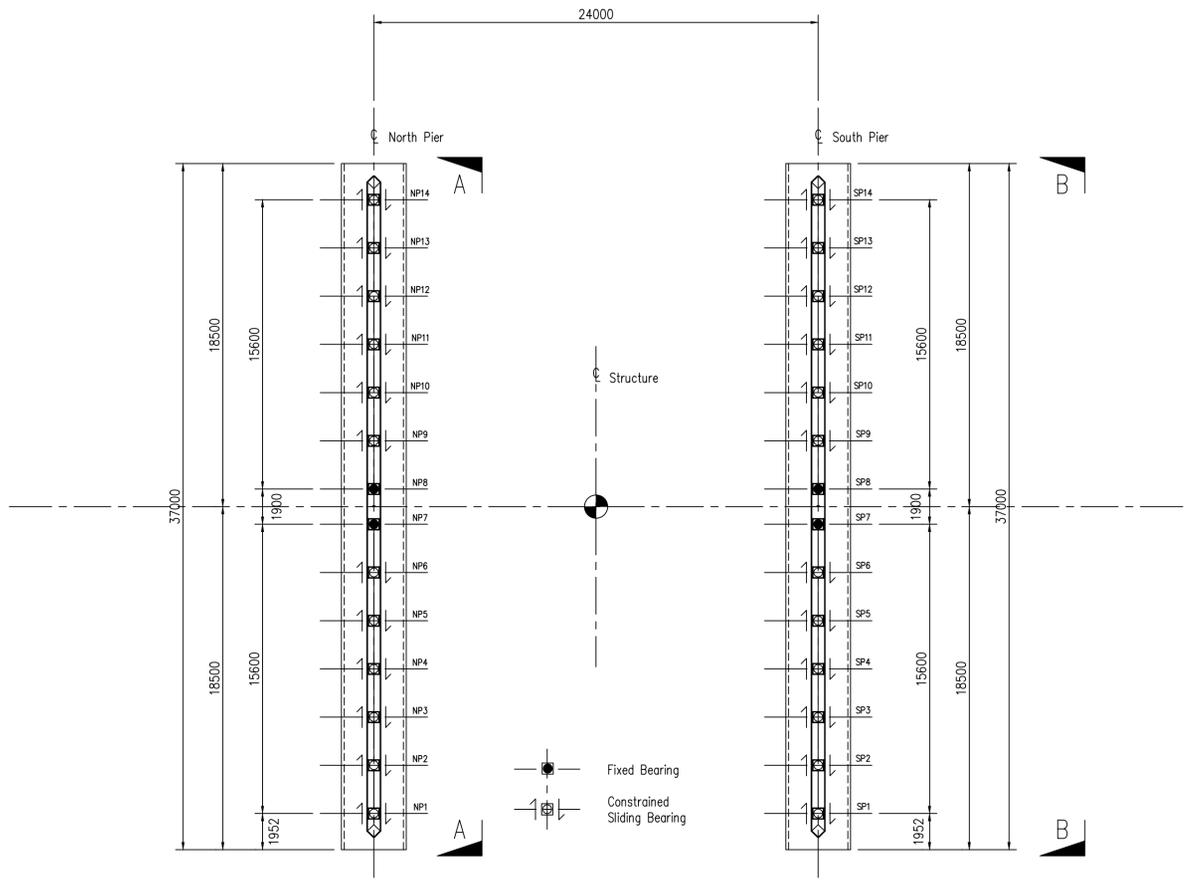
Scale
As Shown

ACAD Ref
hs-deck

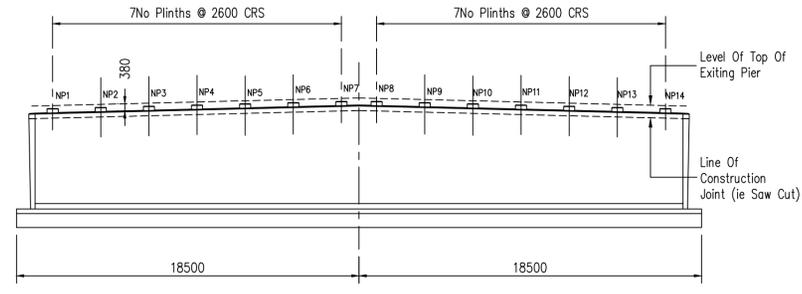
DISC Ref
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Survey Disc Ref

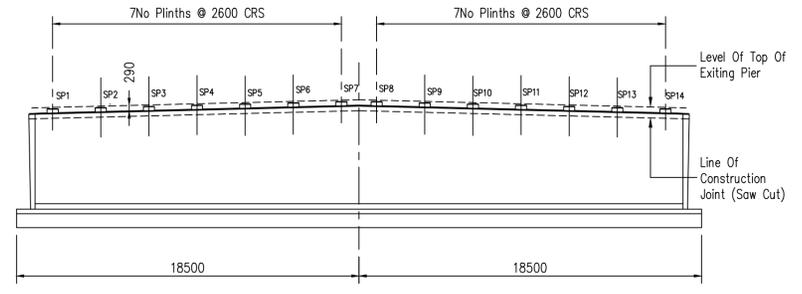
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BP1035.016-0202A



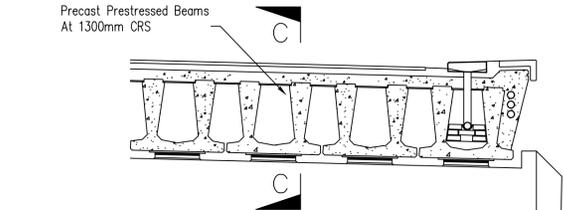
PLAN ON PIERS
1:200



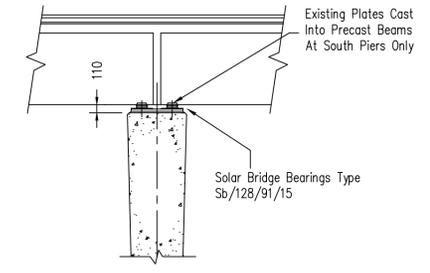
ELEVATION ON A-A
1:200



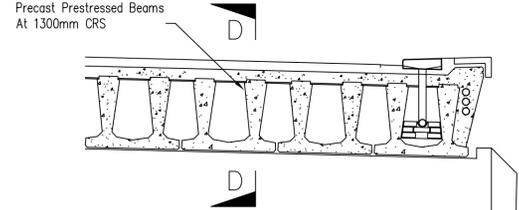
ELEVATION ON B-B
1:200



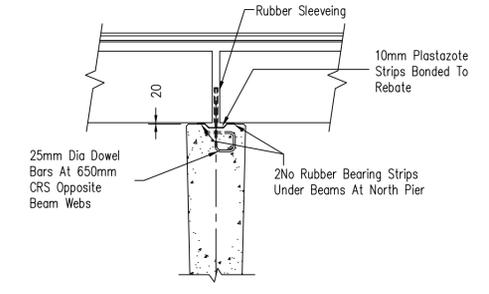
SECTION THROUGH EXISTING DECK
AT EAST END OF SOUTH PIER
1:50



SECTION C-C
1:50



SECTION THROUGH EXISTING DECK
AT EAST END OF NORTH PIER
1:50



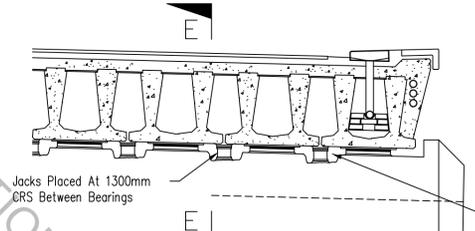
SECTION D-D
1:50

DETAILS OF EXISTING BEARINGS

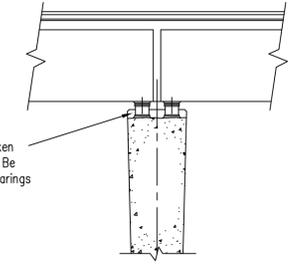
NOTES

- This Drawing Is To Be Read In Conjunction With The Following Drawings BP1035.016-0201 to 0221
- All Bearings To Be Set Horizontally.
- Method Of Replacing Bearings Is For Example Only - Temporary Jacking System To Be Designed By Contractor & Is Subject To Engineers Approval.
- The Jacks Shall Be Placed At Centres Along The Piers No Greater Than Those Of The New Bearings.

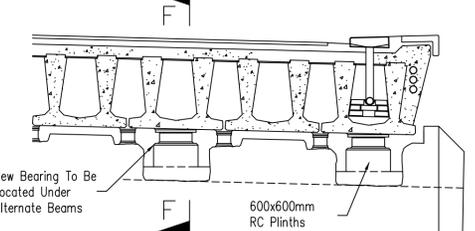
FOR INFORMATION ONLY



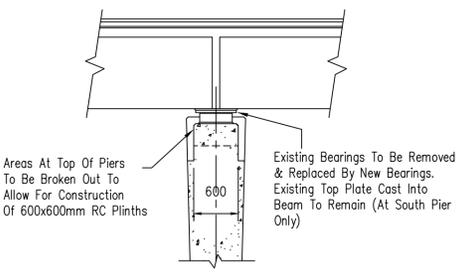
STAGE 1
1:50



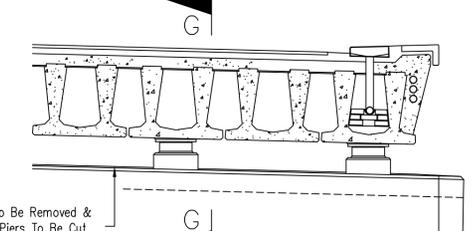
SECTION E-E
1:50



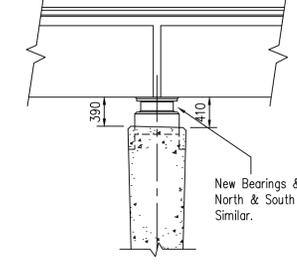
STAGE 2
1:50



SECTION F-F
1:50



STAGE 3
1:50



SECTION G-G
1:50

METHOD OF REPLACING BEARINGS & PARTIAL DEMOLITION

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Client
HIGHWAYS AGENCY

Title
M5 RIVER HUNSPILL BRIDGE
REFURBISHMENT AND BEARING REPLACEMENT
PIER PARTIAL DEMOLITION

designed	PFS	date	Feb 97
drawn	AW	date	Feb 97
checked		date	
approved		date	

Revisions
A : June 97:AW: First Issue
B : March 99:SW: As Built

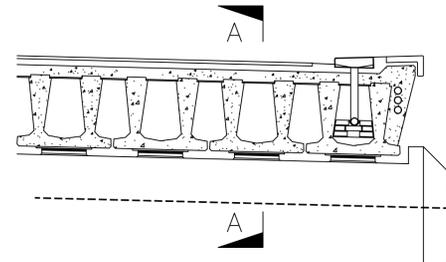
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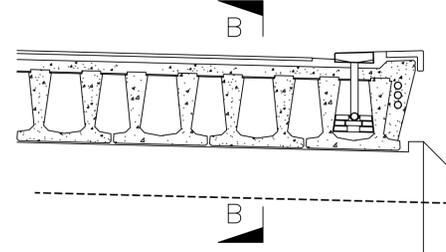
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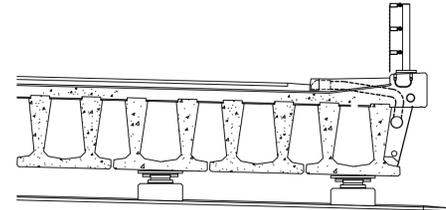
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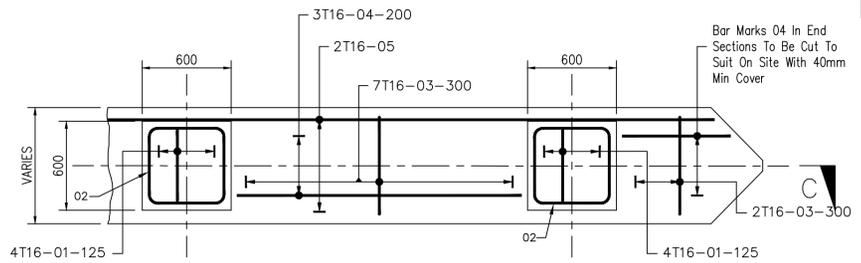
SECTION THROUGH EXISTING DECK AT SOUTH PIER
1:50



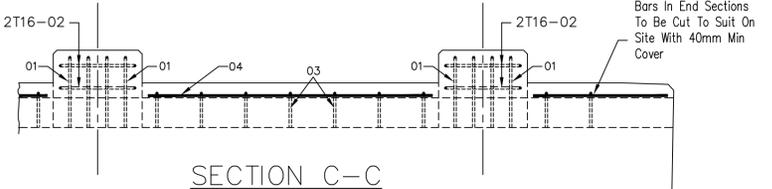
SECTION THROUGH EXISTING DECK AT NORTH PIER
1:50



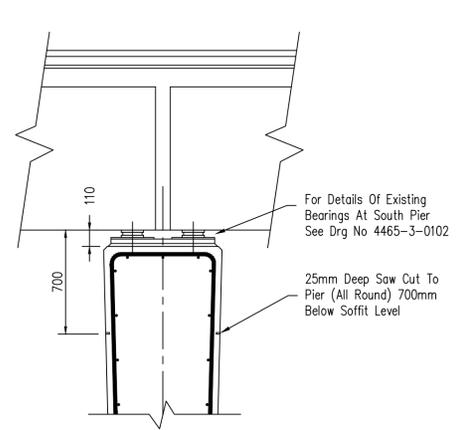
SECTION THROUGH PROPOSED DECK
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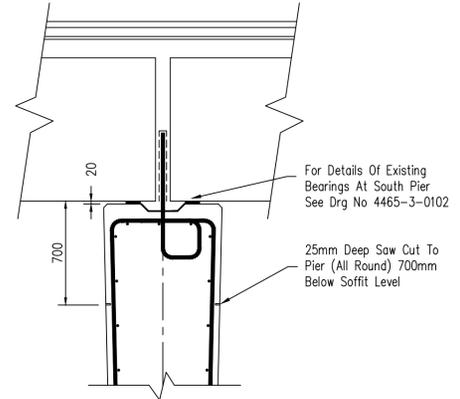
PLAN SHOWING REINFORCEMENT
1:25



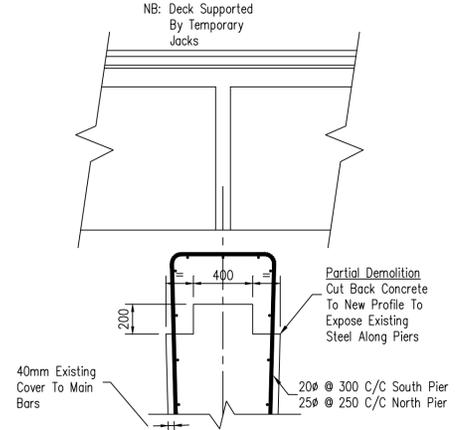
SECTION C-C
1:25



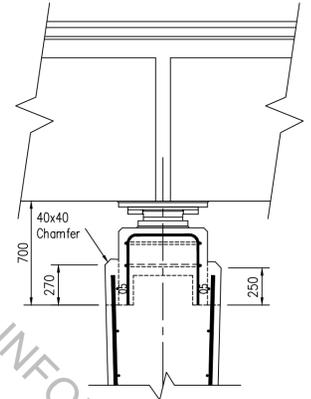
STAGE 1 SECTION A-A
1:25



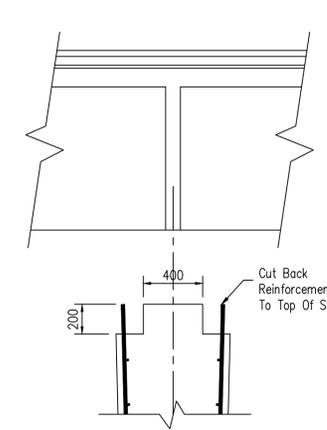
STAGE 1 SECTION B-B
1:25



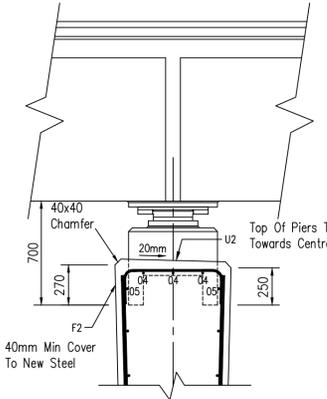
STAGE 2
1:25



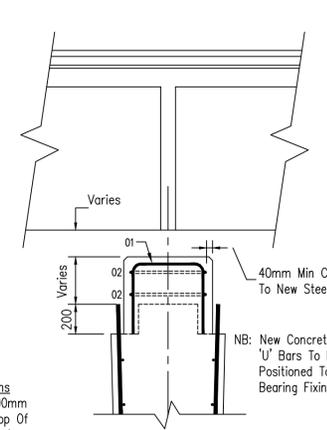
SECTION AT PLINTH
1:25 (28No Thus)



STAGE 3
1:25

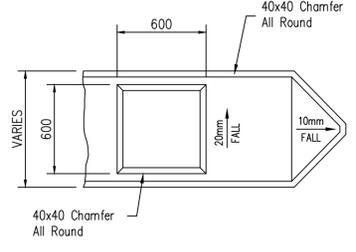


SECTION BETWEEN PLINTHS
1:25

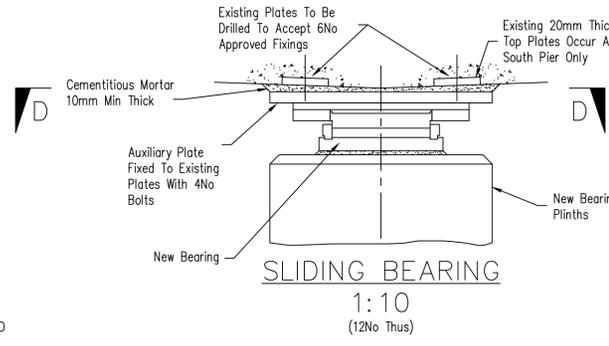


STAGE 4
1:25 (28No Thus)

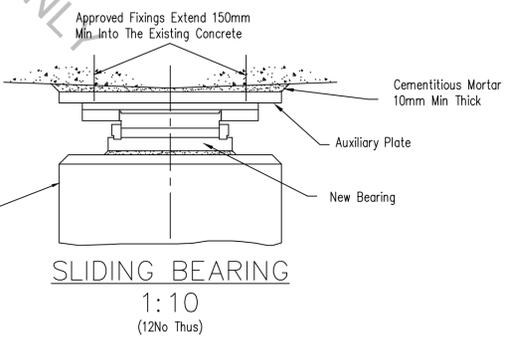
At Bearing Plinths Drill Or Form 200mm Long Holes In Top Of Formed Upstand & Grout In 4No T16 'U' Bars. Construct 600x600mm Plinth In Grade 50 Concrete



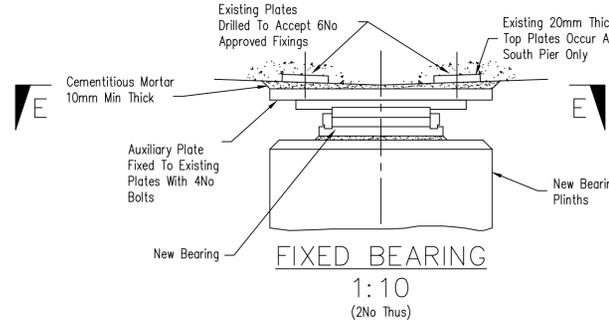
PLAN ON PIER END
1:25



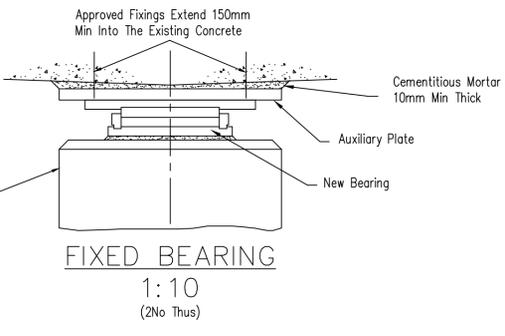
SLIDING BEARING
1:10 (12No Thus)



SLIDING BEARING
1:10 (12No Thus)



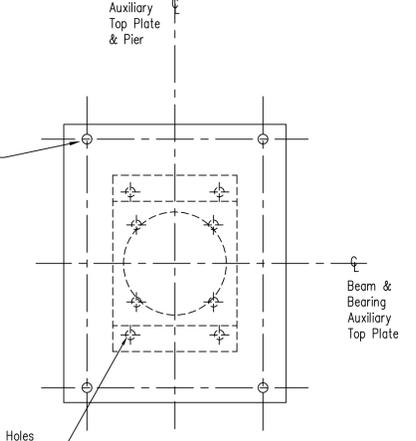
FIXED BEARING
1:10 (2No Thus)



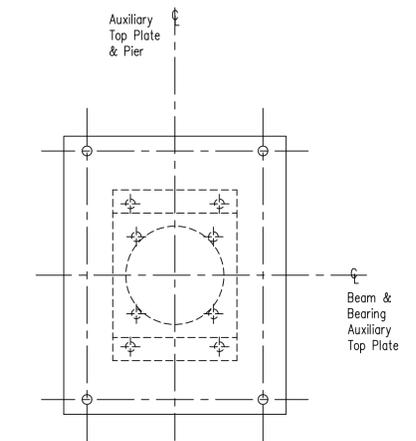
FIXED BEARING
1:10 (2No Thus)

SECTIONS AT SOUTH PIER

SECTIONS AT NORTH PIER



SECTION D-D
1:10 (Beams & Existing Top Plates Omitted For Clarity)



PLAN ON E-E
1:10 (Beams & Existing Top Plates Omitted For Clarity)

NOTES

- This Drawing Shall Be Read In Conjunction With The Following Drawings: BP1035.016-0201 to 0221
- All Bearings & Auxiliary Top Plates Will Be Supplied By Others.
- The Contractor Shall Arrange To Collect The Bearings & Auxiliary Top Plates From The Address Given In Appendix 21/70.
- The Auxiliary Plates Will Be Supplied Without The Fixing Holes Having Been Drilled. The Contractor Shall Determine Suitable Positions For The Auxiliary Top Plate Fixings To Avoid Existing Reinforcement & Then Drill & Grout-In As Appropriate The 4No Fixings Per Plate.
- The Position Of Such Fixings Shall Be Such To Achieve, As Near As Possible, The Centres Shown In Sections D-D & E-E.
- Having Located & Secured The Fixings The Contractor Shall Accurately Drill Each Auxiliary Top Plate To Suit The Fixing Centres.
- Abbreviations : SP - South Pier
NP - North Pier
- This Drawing Is To Be Revised When Exact Dimensions Of Bearings & Auxiliary Top Plates Are Known
- The Contractor shall note that the reinforcement/strands/links in the beams are congested & their position not precisely known. It will be necessary for the Contractor to use different methods of forming holes without damaging the steel in the beams. The position of fixing holes will vary from beam to beam. See Appendix 21/70
- For painting of bearings after installation see Appendix 19/2

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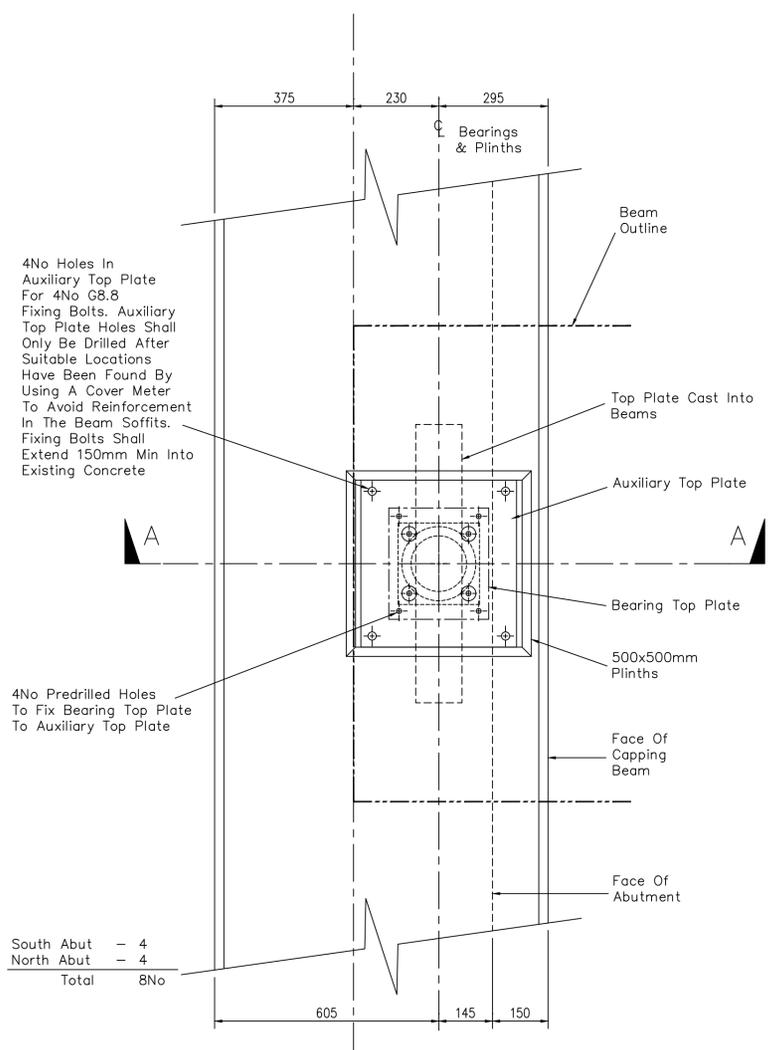
Client
HIGHWAYS AGENCY

Title
M5 RIVER HUNTPILL BRIDGE
REFURBISHMENT AND BEARING REPLACEMENT
PIER BEARING DETAILS

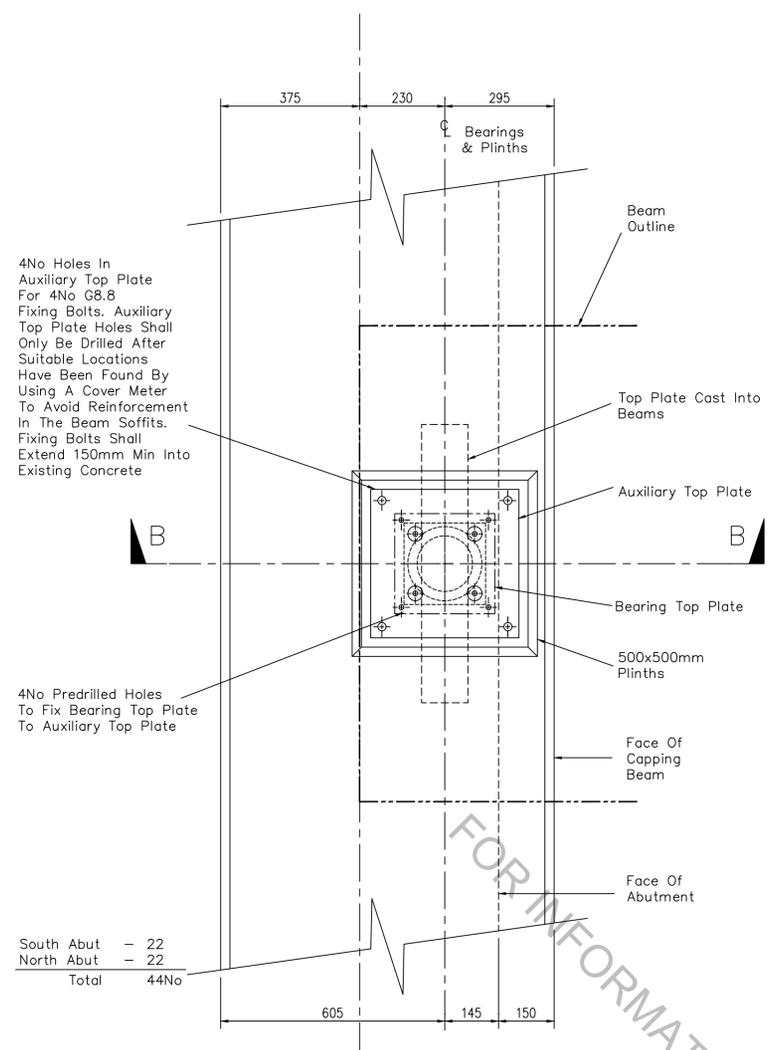
designed	PFS	date	Feb 97
drawn	AW	date	Feb 97
checked		date	
approved		date	

Revisions
A : June 97:AW: First Issue
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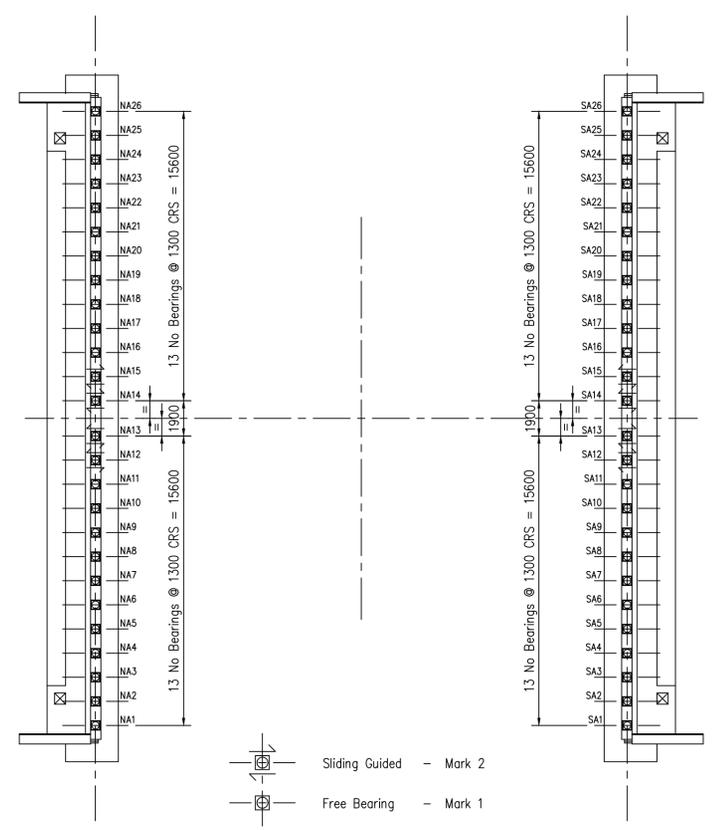
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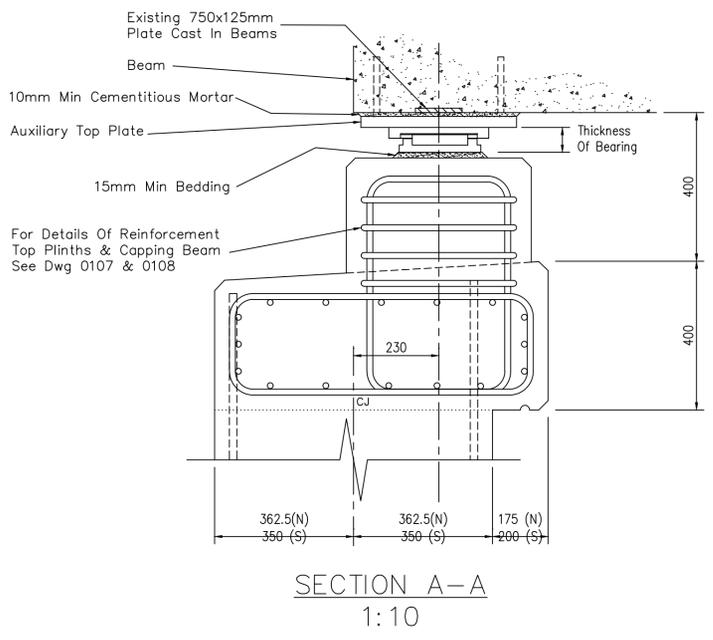
PLAN ON CONSTRAINED SLIDING BEARING MK 2
1:10



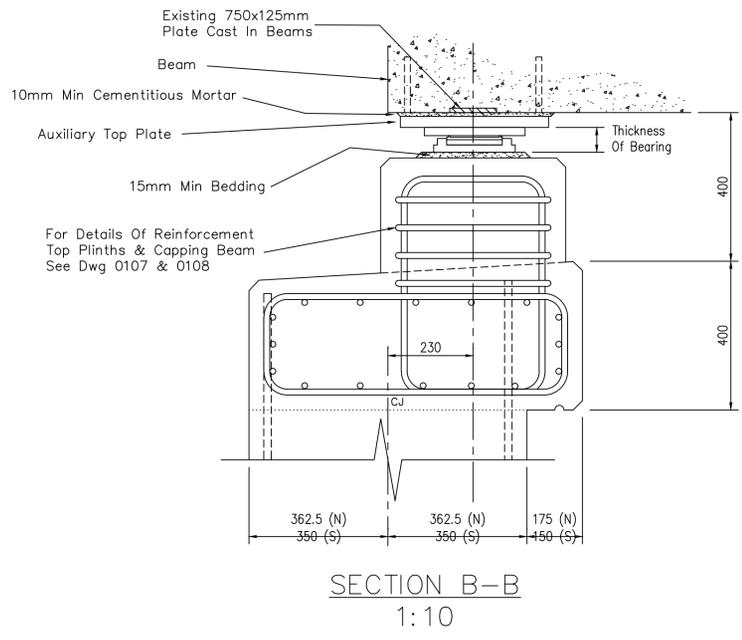
PLAN ON FREE SLIDING BEARING MK 1
1:10



PLAN SHOWING PROPOSED ARTICULATION
1:200



SECTION A-A
1:10



SECTION B-B
1:10

NOTES

- Auxiliary Top Plates To Be The Same Grade Steel As Bearing Top Plate & Are To Receive Similar Paint System As Appendix 19/1
- Fixing Bolts To Be Of Same Material As Supplied For Bearings
- This Drawing Is To Be Read In Conjunction With The Following Drawing:
BP1035.016-0201 to 0221
- All Bearings & Auxiliary Top Plates To Be Supplied By Others
- The Auxiliary Top Plates Are To Be Supplied Without Fixing Holes. The Contractor Shall Determine Suitable Positions For The Auxiliary Top Plate Fixings To Avoid Existing Steel. The Holes Shall Then Be Drilled And The Bolts Grouted In Accordingly
- The Positions Of Fixing Bolts Shall Be Such To Achieve, As Near As Possible, The Centres Shown In Bearing Plans
- Having Located & Secured The Fixings The Contractor Shall Accurately Drill Each Auxiliary Top Plate To Suit The Fixing Centres
- This Drawing Is To Be Revised When Exact Dimensions Of Bearings Are Known
- The Contractor shall note that the reinforcement/strands/links in the beams are congested & their position not precisely known. It will be necessary for the Contractor to use different methods of forming holes without damaging the steel in the beams. The position of fixing holes will vary from beam to beam. See Appendix 21/70
- For painting of bearings after installation see Appendix 19/2

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Client
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Title
**M5 RIVER HUNSTPILL BRIDGE
REFURBISHMENT AND BEARING REPLACEMENT
ABUTMENT BEARING DETAILS**

designed <i>PFS</i>	date <i>Feb 97</i>
drawn <i>AW</i>	date <i>Feb 97</i>
checked	date
approved	date

Revisions
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B : March 99:SW: As Built Drawings

Scale
As Shown

ACAD Ref
hs-abear

DISC Ref
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