



Transport Statement

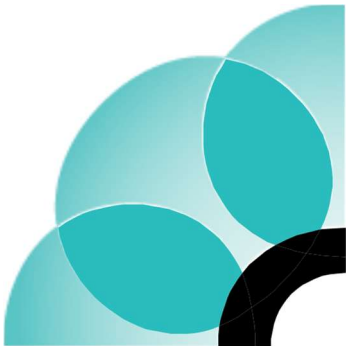
Park Gerry, Roskear, Camborne

On behalf of Camborne Town Council

January 2023

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1.Introduction

This Transport Statement (TS) has been undertaken to identify and analyse the potential transport impacts of proposed improvements at Park Gerry, Roskear, Camborne.

Park Gerry is located within the Roskear area of Camborne. It is an open green space which has been protected by Fields in Trust since 2018 and is one of four parks across the Parish which Camborne Town Council took ownership of in 2017.

The site currently consists of 3 large fields containing a disused football pitch and some hedgerows and trees. It is currently used mainly for dog walking, and until recently for weekly open space activities organized by ParkPlay. There are no formal facilities, parking, pathways or seating.

Brief and scope

The project scope aims to deliver the following infrastructure and landscape enhancements:

- Construction of skatepark and wheeled sports facility;
- Construction of a Multi-Use Games Area (MUGA) plus outdoor gym/activity facilities;
- Creation of 'pavilion' block and space for community hire;
- Re-instatement of full size football pitch;
- Installation of public conveniences;
- Creation of sensory garden;
- Improved, accessible pathways across the site with seating;
- Creation of wildflower meadows with natural play elements;
- Improved dog walking spaces with more robust pathways;
- Provision of sufficient car parking;
- Improved signage and wayfinding;
- Incorporation of cultural and heritage interpretation through artwork and interpretation boards;
- Improved sense of security and safety.

The image below demonstrates the site location within the context of the built development in the

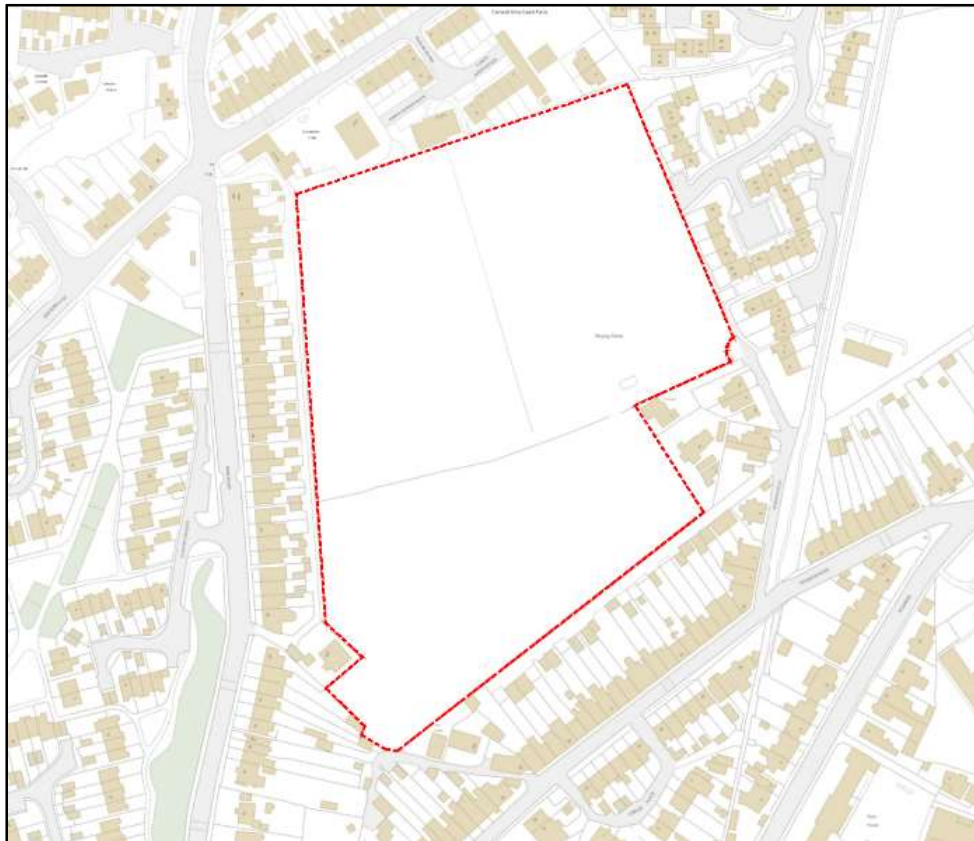
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area.



Site Location

This report will outline and assess any transport issues in relation to the site and will consider the proposed access and parking arrangements of the development with regard to local and national planning policy.

1.1 Overview of Guidance and Standards

A Transport Statement sets out the transport issues relating to a proposed development site (existing conditions) and details of the development proposals (proposed development).

This Transport Statement is produced in accordance with, and in recognition of, local and central

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government guidance and follows our understanding of the requirements set out in the National Planning Practice Guidance (2014) and the National Planning Policy Framework (July 2021).



2.Existing Transport Conditions

This section provides a review of the existing highway conditions at the site in relation to transport. It includes analysis of the site location, a study of the existing local highway network and traffic conditions and includes a road safety review.

2.1 Site Location and Description

Park Gerry is bounded on all sides by existing residential housing. There is an existing vehicular access on the western boundary from Park Road (U6038).

This is currently the main vehicular entrance for park users to an informal parking area and for rear access to the existing residential dwellings on Park Road (U6038). The existing access is demonstrated in the image below:



Existing access from Park Road (U6038)

Park Road is within the existing 20mph speed limit zone and has speed reducing features in the



form of flat top road humps. It is street lit with continuous footways on both sides of the carriageway.

The existing access is located adjacent to the existing double mini roundabout which connects Eastern Lane, Cliff View Road, North Roskear Road and Park Road. This acts as an additional slowing feature as vehicles approach and give way to opposing flows.

Whilst the emerging visibility of the existing access is not ideal, the slow vehicle speeds which were observed during the course of a site visit, and which are enforced by the speed reducing features, ensure that vehicles adhere to the 20mph speed limit.

The emerging visibility of the existing Park Road (U6038) access is demonstrated in the images below, taken from 2.4m back from the edge of the carriageway in accordance with Manual for Streets (MfS) guidance.



Emerging visibility to the right





Emerging visibility to the left

There is also an existing field gate entrance on the eastern boundary from Trenance Road (U6038) that is used for maintenance of the park. This is shown in the image below:



Existing field gate from Trenance Road (U6038)

Trenance Road (U6038) is a cul-de-sac within the 20mph speed limit zone, it is street lit and has

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continuous footways on both sides of the carriageway through the estate.

Trenance Road (U6038) forms a priority junction with North Roskear Road (U6038) to the north. The priority junction and its emerging visibility is demonstrated in the images below:



North Roskear/Tranance Road (U6038) priority junction



Emerging visibility to the right





Emerging visibility to the left

The images demonstrate that the North Roskear/Trenance Road (U6038) priority junction has excellent emerging visibility from 2.4m back from the edge of the carriageway in accordance with MfS guidance for the speed of traffic.

2.2 Road Safety

The five-year personal injury collision statistics were analysed for the whole of Park Road and North Roskear Road (U6038) up to and including its junction with Trenance Road (U6038).

The data shows there were no personal injury collisions in the area analysed, it can therefore be concluded that there is no evidence of an existing road safety issue. This accords with the site visit assessment of a very low speed environment that reduces the potential for vehicular and pedestrian conflict.

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2.3 Pedestrian and Cycle Access

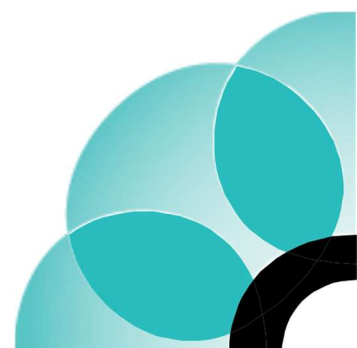
Park Gerry is located in the heart of Camborne and therefore very accessible to many local residents. Manual for Streets (MfS) states that “walkable neighbourhoods are typically characterised by having a range of facilities within 10 minutes’ (up to about 800m)” and identifies that “walking offers the greatest potential to replace short car trips, particularly those under 2km.”

The location of Park Gerry is very accessible to the residents of Camborne who would likely walk or cycle to make use of the facility.

2.4 Public Transport

The site is a short walk to existing bus stops and Camborne Bus Station. There are regular bus services around Camborne and these also provide connections between other towns, villages and hamlets in west Cornwall.

Public transport is therefore a realistic option for future users of Park Gerry.



3. Policy and Guidance

3.1 National Planning Policy Framework (NPPF)

Relevant current government guidance is contained in the National Planning Policy Framework (NPPF) 2021. This promotes development in locations that are accessible by means other than the private car.

Chapter 9 of the NPPF is titled “Promoting sustainable transport” in which the principles of sustainable transport in relation to development are promoted. It states in paragraph 104 that:

Transport issues should be considered from the earliest stages of plan-making and development proposals, so that:

- a) the potential impacts of development on transport networks can be addressed;
- b) opportunities from existing or proposed transport infrastructure, and changing transport technology and usage, are realised – for example in relation to the scale, location or density of development that can be accommodated;
- c) opportunities to promote walking, cycling and public transport use are identified and pursued;
- d) the environmental impacts of traffic and transport infrastructure can be identified, assessed and taken into account – including appropriate opportunities for avoiding and mitigating any adverse effects, and for net environmental gains; and
- e) patterns of movement, streets, parking and other transport considerations are integral to the design of schemes, and contribute to making high quality places.

It also states in paragraph 105:

The planning system should actively manage patterns of growth in support of these objectives. Significant development should be focused on locations which are or can be made sustainable,



through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health. However, opportunities to maximise sustainable transport solutions will vary between urban and rural areas, and this should be taken into account in both plan-making and decision-making.

Furthermore, Paragraph 110 and 111 states:

In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- a) appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location;
- b) safe and suitable access to the site can be achieved for all users;
- c) the design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code⁴⁶; and
- d) any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

111. Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Finally, paragraph 38 of the NPPF details the new approach to planning:

Local planning authorities should approach decisions on proposed development in a positive and creative way. They should use the full range of planning tools available, including brownfield registers and permission in principle, and work proactively with applicants to secure developments that will improve the economic, social and environmental conditions of the area. Decision-makers at every level should seek to approve applications for sustainable development where possible.



3.2 Cornwall Council Policy

The Cornwall Transport Plan Local Transport Plan to 2030

The Cornwall Transport Plan is the key strategic policy tool through which Cornwall Council exercises its responsibilities for planning, management and development of transport in Cornwall, for the movement of both people and goods: “Policy 13: We will use the planning process to ensure that development proposals are sustainable and include quality walking and cycling infrastructure with direct links to neighbouring communities, services and transport facilities and integration with existing rights of way network.”

The Cornwall Transport Plan also states that “We will ensure that all new development is designed to prioritise and encourage people to walk and cycle safely and easily.”

3.3 Cornwall Local Plan

Policy 27: Transport and accessibility

All developments should:

Provide safe and suitable access to the site for all people and not cause a significantly adverse impact on the local or strategic road network that cannot be managed or mitigated.

For major developments to ensure a resilient and reliable transport system for people, goods and services, development proposals should:

Be consistent with and contribute to the delivery of Connecting Cornwall 2030, Cornwall’s Local Transport Plan or any subsequent LTPs; and

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Locate development and /or incorporate a mix of uses so that the need to travel will be minimised and the use of sustainable transport modes can be maximised by prioritising safe access by walking, cycling and public transport to minimise car travel; and

Locate larger developments which attract a proportionally larger number of people in the city and main towns or locations which are highly accessible by public transport. Any proposals which do not accord with this will require significant justification and provide clear transport benefits; and

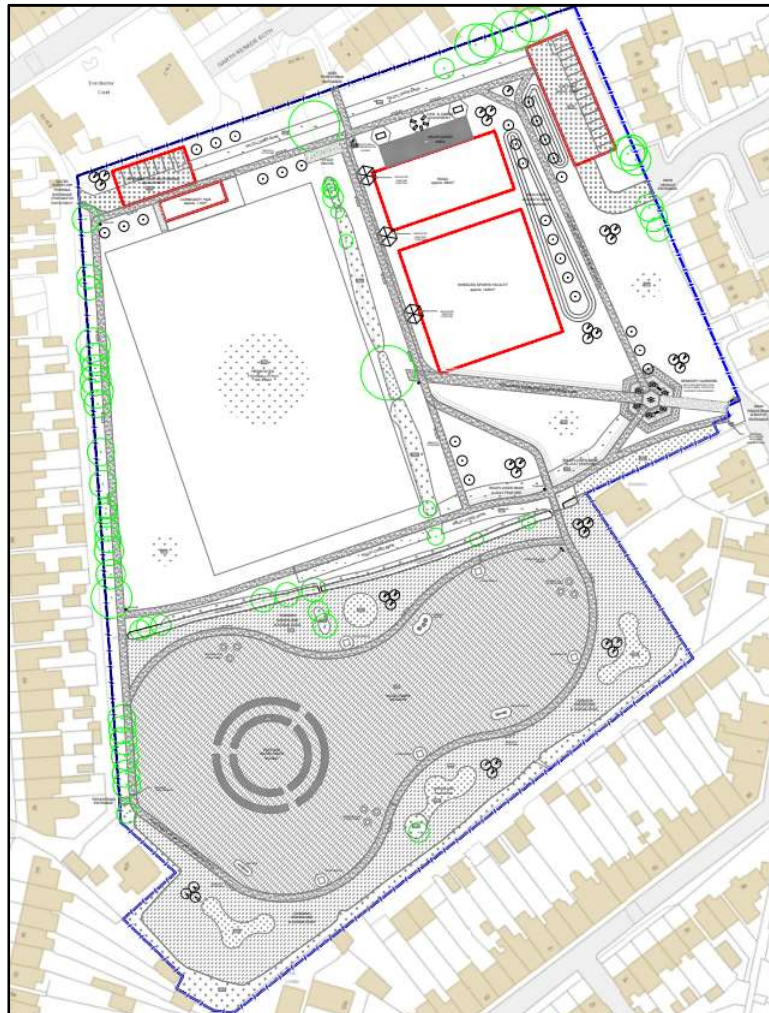
Be designed to provide convenient accessible and appropriate cycle and pedestrian routes, public transport and road routes within and in the immediate vicinity of the development.



4. Proposed Development

4.1 Access and Layout

The proposed site layout is demonstrated in the image below and is taken from the general arrangement plan (DR-L-1002 Rev01) which has been submitted and accompanies the planning application:

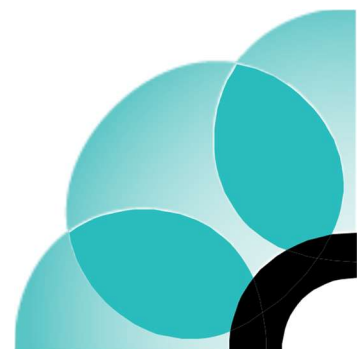


Proposed site layout

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It is proposed as part of the improvements, to make the main entrance to Park Gerry from Trenance Road (U6038) on the eastern boundary. This will be achieved at the existing field gate entrance which will be widened so that vehicles can pass each other in opposing directions. The entrance will also be gated and open into Park Gerry.

It is proposed that 14 car parking spaces including 1 disabled space are provided at Park Gerry, accessed from the new main entrance from Trenance Road (U6038).

The existing vehicular access from Park Road (U6038) to the houses can be used for secondary access.

The proposal includes additional pedestrian and cycle access points around the site and the provision of a link to the old tramway to encourage and facilitate the accessibility of the site to the surrounding residential properties wherever they are located in relation to Park Gerry.

4.2 Traffic Generation

Due to the site location in the heart of Camborne's residential area and that it will be people who live locally who will visit and benefit from the improvements most, the vast majority of users of Park Gerry are clearly going to come on foot or bicycle.

The proposal does include the re-instatement of a full size football pitch and it is considered that of all the proposed improvements on Park Gerry, this has the potential to generate the most vehicular traffic. It is therefore considered appropriate to look at the likely traffic generation on match days, as this will generate the most intensive traffic movements compared with anything else.

It is envisaged that the pitch will be used by a local league team and whilst you would expect the home players to live locally and therefore walk or cycle on match days, the away team and any match officials would arrive by car.

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It is likely that visiting teams will car share with 2 or 3 players per car. Using first principles, for a team of 11 players and 5 substitutes this would equate to between 4 and 8 cars plus a car containing the match officials. It is therefore considered robust to assume that the maximum number of vehicles generated by the away team and officials on a match day would be 9 cars.

This still leaves 4 standard car parking spaces which would be available for any vehicles associated with the home team, who as advised previously in this report would live locally and not require a car to play home matches.

To further control the car parking and ensure that there is sufficient parking on site, a parking barrier for match day only provision will be issued along with the changing room keys.

It is therefore considered that the parking provision of 14 car parking spaces which includes a disabled space, is sufficient to accommodate the parking requirements on match days.

It should also be noted that there has previously been a football pitch at Park Gerry and that this could have been brought back into use and its associated traffic generation without requiring planning permission.

The likely traffic generation analysis for the proposal demonstrates that at its busiest, which is considered to be on match days, the number of vehicular trips generated by the proposal is negligible and can easily be accommodated on the surrounding highway network and will have no discernible impact on the local highway network or the free flow of traffic.



5. Conclusions

This Transport Statement has assessed the traffic impacts from the proposed improvements at Park Gerry, Roskear, Camborne.

Park Gerry would be served from an improved entrance from Trenance Road (U6038) which would become the main entrance and provide access to the proposed designated parking area.

It has been demonstrated within this report that the emerging visibility of the North Roskear/Trenance Road (U6038) priority junction is excellent and in accordance with guidance contained within Manual for Streets for the speed of traffic.

Providing the main entrance to Park Gerry from Trenance Road (U6038) is considered a potential highway safety improvement in comparison to the existing Park Road (U6038) access.

The reinstatement of the football pitch is likely to generate most traffic movements on match days and this report has demonstrated using first principles, that the likely number of vehicular trips generated by the proposal is negligible.

The proposed site layout demonstrates that vehicles will be able to enter and exit Park Gerry in a forward gear and that the parking provision is appropriate and sufficient when traffic movements would be greatest on football league match days.

Park Gerry is very accessible to the residents of Camborne who would likely walk or cycle to make use of the facility. Additional pedestrian and cycle access points around the site are proposed to increase the accessibility of Park Gerry to residents.

Camborne has regular bus services throughout the town, therefore public transport is a realistic option for future users of Park Gerry.

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It has been identified that there have been no collisions on the highway network within the vicinity of Park Gerry, which demonstrates that there are no existing highway safety issues.

Therefore, it is concluded that this proposed development is appropriate and acceptable in traffic and transport terms. The proposal is compliant with both Local and National Policy and accords with Manual for Streets guidance.

Based on the above, it is considered that the proposed development is acceptable and that safe and suitable access has been demonstrated.

